

Glismann Road Area Development Plan Development Plan Overlay Schedule 19

November 2025 (Amended 18 May 2026)

Version history	
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Version	Date	Comment	Author
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B	6 November 2025	Final Development Plan	UDM/CSC
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Urban Design and Management

In collaboration with:

Cardinia Shire Council

Acknowledgement of Country

Cardinia Shire Council wishes to acknowledge the Bunurong and Wurundjeri peoples as the Traditional Owners of the land that makes up Cardinia Shire and pays respect to their Elders, past and present.

Cardinia Shire’s name is derived from the Bunurong word ‘kar-din-yarr’, meaning ‘look to the rising sun’ or ‘close to the sunrise’. Council’s logo, which includes a motif of the rising sun, reflects this meaning.

Cardinia Shire Council acknowledges the right of Indigenous Australians, and indeed all Australians, to live according to their values and customs, subject to the law.

Council is committed to developing and strengthening relationships through reconciliation and supports the reconciliation process, which promotes mutual respect and understanding of the indigenous people and of all ethnic groups and their history and culture in our community.

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Table 1 - Glossary of Terms used

Term	Definition
Concept plan	A concept plan is a preliminary illustration or layout that outlines the general design and key elements of a proposed development or infrastructure project. It is typically used in the early stages of planning to visualise ideas and explore potential design options. Concept plans are not final or binding, they are subject to change and refinement through consultation, approvals, and detailed design processes with the Responsible Authority.
Conventional density	Residential lots generally greater than 300sqm and less than 700sqm, suitable for a single dwelling and garage.
CSC	Cardinia Shire Council
DCP and DCP05	Development Contributions Plan (Glismann Road Development Contributions Plan, Urban Enterprise, May 2022, Amended December 2023) Development Contributions Plan Overlay Schedule 5
DCP Project Sheets	The project costs are identified in the DCP. This document contains the project sheets for the infrastructure items in the DCP and the methodology used for land valuations. Glismann Road Development Contributions Plan Project Sheets, Cardinia Shire Council, November 2022
DPO19	Development Plan Overlay Schedule 19
DTP	Department of Transport and Planning
ES01	Environmental Significance Overlay Schedule 1
FLP	Functional Layout Plan
GRADP (also DP)	Glismann Road Area Development Plan
GRZ1	General Residential Zone Schedule 1
High Overall Landscape Values	High Overall Landscape Value areas as mapped in the 2014 Hansen Partnership Landscape Assessment - <i>High landscape value is associated with the Small Acreage: Bushland areas, which are scattered throughout the study area. This level of relatively high value was largely assigned due to the presence of groups of well established native vegetation.</i>
IDM	Infrastructure Design Manual
KIP	GRADP Key Issues Paper (2025)
LMP	Land Management Plan
Medium density	Residential lots less than 300sqm in size, and capable of being developed for smaller dwellings, units or townhouses.
MSA	Melbourne Strategic Assessment area
Neighbourhood Residential	Proposed land use - conventional and medium density lots on less constrained land, integrating with surrounding development while retaining significant vegetation.
NRZ2	Neighbourhood Residential Zone Schedule 2
OSLP-01	Glismann Road DCP Project – Local open space embellishment

Term	Definition
RD-01	Glismann Road DCP Project - Glismann Road construction - Access Street Level 2 cross section from Old Princes Highway to TM-01.
RD-02	Glismann Road DCP Project - Glismann Road part construction - Access Street Level 1.5 cross section from TM-01 to truncation of road.
RD-03	Glismann Road DCP Project – Local Access Street Level 1 – 6 Glismann Road (west loop road)
RD-05	Glismann Road DCP Project – Local Access Street Level 1 – 1 Glismann Road (east loop road)
Referral / Servicing Authorities	<p>These agencies provide advice or conditions, such as:</p> <ul style="list-style-type: none"> • Melbourne Water – for floodplain management, drainage, and waterway protection. • South East Water – for water supply and sewerage infrastructure. • APA Group / AusNet Services – for gas and electricity infrastructure.
Responsible Authority	Typically, Cardinia Shire Council is the responsible authority for land within its municipality. DTP is the responsible authority for Old Princes Highway and section of Glismann Road within proximity to the signalised intersection.
SMP	Slope Management Plan
TM-01	Glismann Road DCP Project – Roundabout or alternative treatment to the satisfaction of the Responsible Authority at Glismann Road RD-02 and RD-03.
Visually sensitive area	<p>Areas identified on the KIP Design Response Plan that are in:</p> <ul style="list-style-type: none"> • Areas of Steep or Extremely Steep Slope. • Proximity to the ridgeline. • Have high overall landscape values.
Visually Sensitive Residential	Proposed land use - larger lots on steeper or visually prominent land, designed to minimise visual impact and earthworks and protect landscape and vegetation.

1. Introduction

Cardinia Shire Council has engaged Urban Design and Management (UDM) to prepare this Glismann Road Area Development Plan (GRADP) in accordance with the requirements of [Schedule 19 to Clause 43.04 of the Cardinia Planning Scheme](#). It applies to all landholdings shown in Figure 1.

The GRADP was approved by Council at its meeting on the 17 November 2025. The Minutes of the Council Meeting are provided as Attachment A.

The GRADP provides a coordinated framework for the future subdivision and development of land within DPO19. It establishes a clear basis for the integrated planning and delivery of:

- Access and the internal road network.
- Management of slope and topography.
- Infrastructure and development staging and delivery.
- Vegetation retention and landscape character.
- Built form and urban design outcomes.
- Open space provision.

A permit granted to use or subdivide land, construct a building or construct or carry out works must be generally in accordance with the GRADP and any other applicable planning control including the Neighbourhood Residential Zone Schedule 2 (NRZ2), General Residential Zone Schedule 1 (GRZ1, applicable to 11 Mahon Avenue), the Development Contributions Plan Overlay Schedule 5 (DCPO5) and the Environmental Significance Overlay Schedule 1 (ESO1). The GRADP is also included in the Melbourne Strategic Assessment area (refer to Section 0 for further discussion).

1.1. Purpose of the GRADP

The purpose of the GRADP is to provide an integrated framework to guide the coordinated subdivision and development of land within the DPO19 area. The GRADP ensures that development proceeds in an orderly and logical manner, supported by the timely provision of infrastructure, protection of environmental and landscape values, and the achievement of a high-quality built form and public realm. It also provides guidance to landowners, developers, and Council on the expectations for future development and facilitates consistent assessment of planning permit applications.

1.2. Document Structure

Key sections of the GRADP are as follows:

- Section 2 – Site Analysis: With reference to the GRADP Key Issues Paper (KIP), provides an overview of the existing site conditions with particular reference to topography and key constraints and opportunities.
- Section 3 – Design Response: With reference to the KIP, outlines the principles and strategies guiding the overall design approach for the GRADP.
- Section 4 – Development Plan: Presents the proposed GRADP including vision and objectives.
- Section 5 – Movement Network: Sets out the road hierarchy, pedestrian and cycling connections, and public transport considerations.
- Section 6 – Land Use Typologies: Establishes preferred land use typologies in accordance with the design principles established in DPO19.
- Section 7 – Building Design Guidelines and Fencing Controls: Provides guidance as required by DPO19 on building form, setbacks, and fencing to ensure visual amenity and consistency.
- Section 8 – Slope Management Guidelines: Provides guidance as required by DPO19 for managing development on sloping land, including cut, fill, and retaining structures.
- Section 9 – Vegetation: Provides guidance on tree retention, habitat zones, scattered trees and areas of high landscape value.
- Section 10 – Landscape: Guides streetscape and open space landscaping to reinforce the precinct's character.
- Section 11 – Integrated Water Management: Sets out stormwater, drainage, and water sensitive urban design requirements.
- Section 12 – Utilities: Provides guidance on servicing, including electricity, gas, sewer, and telecommunications.
- Section 13 – Environmental Site Assessments: Summarises further site contamination investigations for Property 1, 4, 5 and 16. Table 3 provides the property address for the Property numbers identified.
- Section 14 – Development Coordination: Provides guidance for future development sequencing and infrastructure delivery.
- Section 15 – Land Budget: As provided in the Glismann Road Development Contributions Plan (DCP).
- Section 16 – Individual Property Guidance: Provides property specific guidance for individual properties.

With reference to Clause 4.0 of DPO19, Table 2 summarises where these are addressed in the GRADP.

Table 2 - DPO19 References

Requirement	Where addressed in the GRADP
<p>Guide development GRADP Vision statement, objectives, requirements and guidelines.</p>	Section 4.
<p>Site analysis plan Responds to Background Reports, identifies key interface areas, identifies visually prominent hilltops and hillsides.</p>	<p>Sections 2 and 3. Also refer to KIP and additional background reports identified at Section 1.4.</p>
<p>Slope Management Guidelines Slope Management Guidelines, Slope Management Design Principles, and a statement of what works outlined in the Slope Management Plan will need to be undertaken by the developer prior to the issues of the Statement of Compliance.</p>	Sections 8 and 14.
<p>Staging An indicative lot layout, design guidelines for buildings and fencing, and a housing capacity analysis that assesses how affordable housing will be distributed throughout the site and how the proposed mix and type of housing responds to local housing needs.</p>	Sections 6 and 7.
<p>Vegetation, Landscape and views A landscape master plan.</p>	Sections 9 and 10.
<p>Public open space</p>	Section 4.
<p>Road network and movement</p>	Section 5.
<p>Integrated water management and utilities An integrated water management plan (includes a levee bank of 450 mm along the frontage of the four existing properties fronting Old Princes Highway), and an infrastructure plan</p>	Sections 11 and 12.

1.3. Land to which the GRADP applies

The GRADP applies to all land included in DPO19 as shown in Figure 1. The GRADP is located on the traditional lands of the *Bunurong* tribe, part of the *Kulin* nation.

The Property References shown in Table 3 are consistent with those used in the DCP with the following exceptions:

- Property Reference 21 which is 21-R.
- Property Reference 22 is not included in the Development Contributions Plan but is included in the Development Plan. For further information regarding the exclusion of Property Reference 22 can be found in the [Panel Report for Cardinia Planning Scheme Amendment C238card](#).

For clarity and consistency, the Property Reference number will be used throughout the GRADP to identify properties instead of the legal property address or street number.

Table 3 - GRADP Property and Address References

Property Reference	Address
1	2 Glismann Rd
2	4 Glismann Rd
3	6 Glismann Rd
4	8 Glismann Rd
5	10 Glismann Rd
6	12 Glismann Rd
7	14 Glismann Rd
8	16 Glismann Rd
9	15 Glismann Rd
10	13 Glismann Rd
11	11 Glismann Rd

Property Reference	Address
12	9 Glismann Rd
13	7 Glismann Rd
14	5 Glismann Rd
15	3 Glismann Rd
16	1 Glismann Rd
17	111-113 Old Princes Hwy
18	115-117 Old Princes Hwy
19	119-121 Old Princes Hwy
20	123-125 Old Princes Hwy
21	Glismann Road - Road Reserve
22	11 Mahon Ave

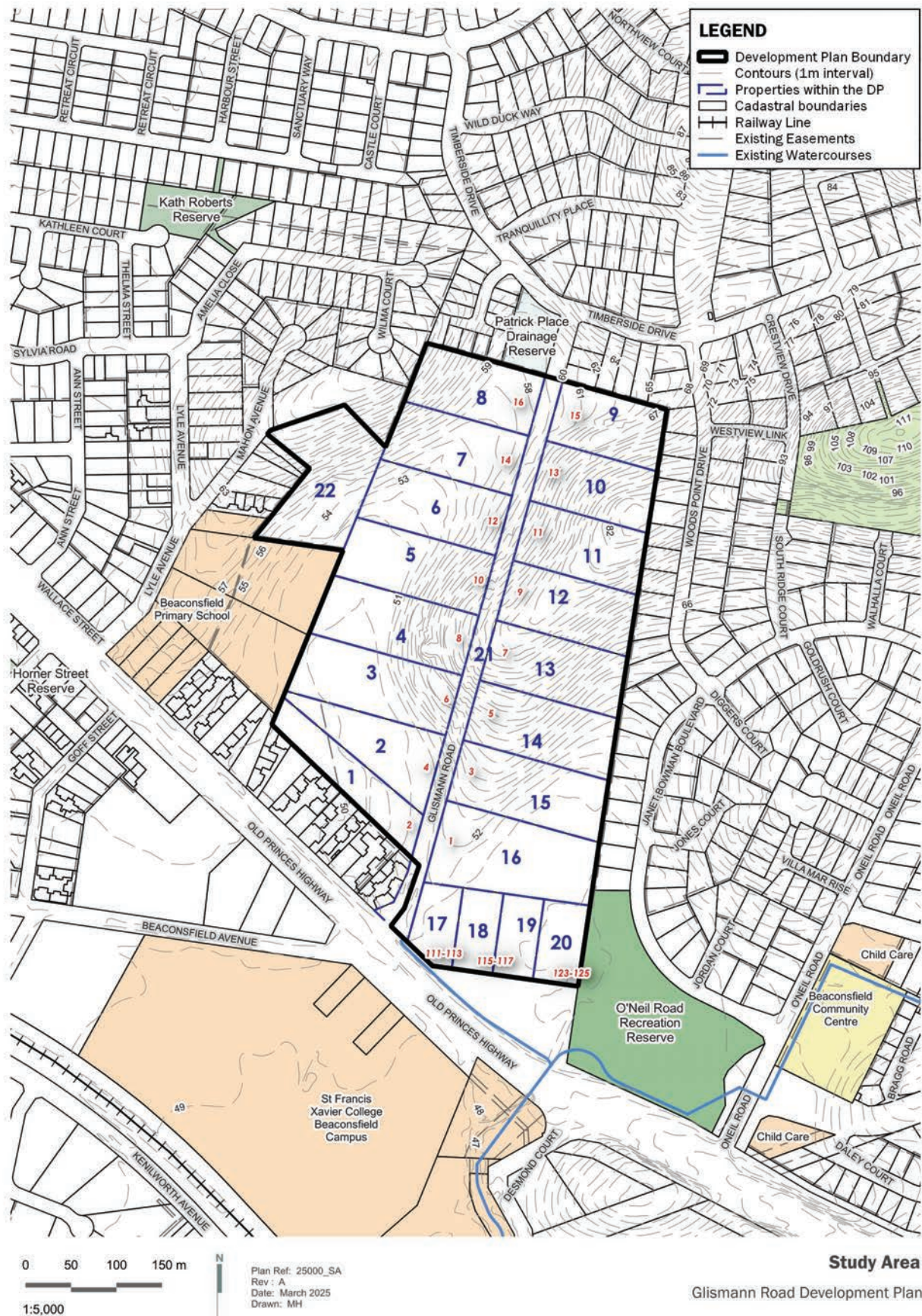


Figure 1 - GRADP Study Area

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1.4. Background Documents

The GRADP builds on a complex planning history spanning over 20 years. Supporting documents accompanying the GRADP include the *GRDP Key Issues Paper (CSC, October 2025)* and the *GRDP Engagement Outcomes Report (CSC, October 2025)*.

The Key Issues Paper identifies and analyses the key planning issues, opportunities and constraints relevant to the GRADP. It also summarises specialist reports and other supporting documents primarily prepared for Planning Scheme Amendment C238card. Importantly, the Key Issues Paper identifies a response and recommended actions for each identified issue, indicating matters to be addressed through the GRADP and, where appropriate, to be included as conditions on any future planning permit.

The GRADP should be read in conjunction with the *GRDP Key Issues Paper (KIP)*, the *GRDP Engagement Outcomes Report*, supporting technical investigations and reports (all available on Council's website), and the relevant provisions of the Cardinia Planning Scheme, including DPO19.

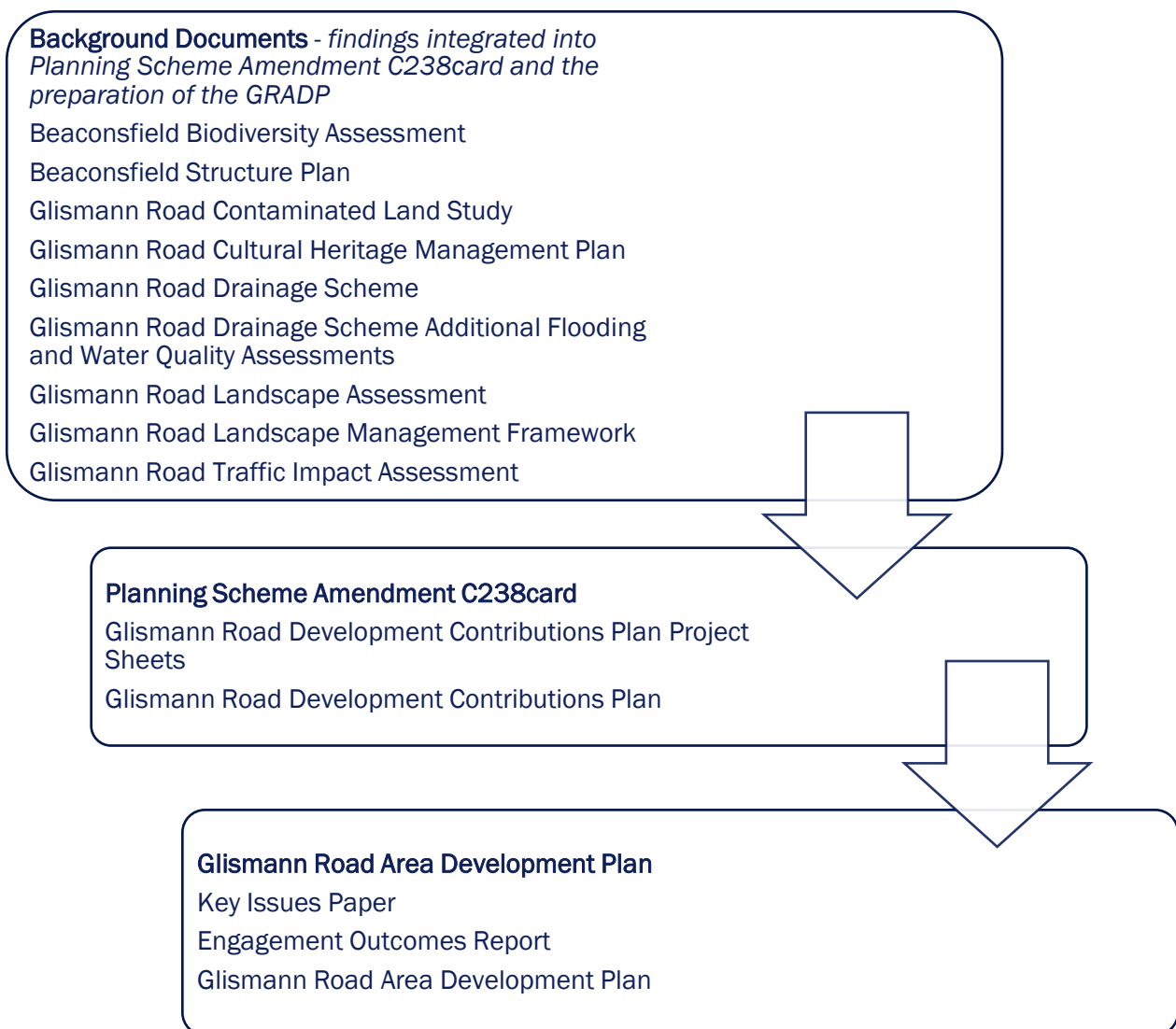


Figure 2 - GRADP Background Documents and Development History

1.5. Generally in Accordance Guidelines

Ensuring that permit applications are generally in accordance with the GRADP provides consistency and protects its long term vision.

In accordance with Clause 43.04 of the Cardinia Planning Scheme, a permit granted must:

- Be generally in accordance with the GRADP.
- Include any conditions or requirements specified in DPO19.

The term 'generally in accordance' represents a threshold test that all permit applications must satisfy. Meeting this test does not imply permit approval, a planning merits assessment against all other relevant clauses is required to determine whether an acceptable planning outcome will be achieved.

The Victorian Planning Authority (VPA) [Generally in Accordance Guidelines](#) (*Generally in Accordance: Guidance Note (VPA, April 2020)*) provide a practical framework to guide decision making, including:

- Judge each matter on its facts and circumstances.
- Make decisions in the context of the whole plan (e.g. Development Plan or a Precinct Structure Plan).
- Make decisions in the context of the objectives to be achieved.
- A matter can be in general accordance without being identical to the plan.
- Accord less flexibility to matters resolved to a higher level of detail in the plan.
- Accord greater flexibility on matters resolved in less detail in the plan.
- In acknowledgement that some land within the GRADP may be used for residential lifestyle purposes prior to its redevelopment, minor works and structures (such as sheds and sealed driveways) may be considered to be generally in accordance with this approved development plan, provided they do not prejudice the orderly future development of the land as anticipated by the approved development plan.

1.6. Permits Issued

Permits issued within DPO19 must not adversely affect the amenity of the area or compromise the future development of land as outlined in this GRADP. This requirement is particularly important for items funded through DCP05, as ensuring coordination with these works is essential to delivering its intended outcomes.

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2. Site Analysis

To support the guidance provided in the GRADP, it is important to understand its key characteristics and constraints. The KIP provides detailed analysis of the site, including topography, vegetation, and existing development patterns. The following plans and observations draw on that analysis to provide a visual and descriptive overview of the site.

These plans are positioned at the very front of the GRADP to give readers immediate insight into the planning issues and design guidance that follow.

By summarising key elements such as slope, ridgelines, vegetation, and existing development, the plans provide a foundation for informed decision-making on building siting, lot layout, infrastructure placement, and design guidelines throughout the area.

2.1. Site Analysis Plan

The Site Analysis Plan (Figure 3) is reproduced from the KIP and summarises the existing key spatial and environmental characteristics of the GRADP. It provides a visual reference to support the analysis and discussion of planning issues in this document.

Key observations include:

- **Topography:** The site contains significant variation in slope, with ridgelines in the northeast and west, steep to very steep slopes alongside ridges, and flatter land in the valley areas.
- **Vegetation:** Existing native vegetation is concentrated along ridgelines, slopes, and drainage lines. Many areas of high landscape value are identified, contributing to the visual character of the site.
- **Existing Development:** Dwellings along Glismann Road are typically sited along ridgelines or immediately below to maximise views. Lots fronting Old Princes Highway are smaller.
- **Access and Road Network:** The area is currently accessed via Glismann Road, with 3 properties having their current access from Old Princes Highway. Current access points to the properties are influenced by topography and existing dwellings.
- **Community Facilities:** The O'Neil Road Recreation Reserve and nearby Beaconsfield Primary School provide important community connections and opportunities for future public open space interfaces and connections.
- **Visual and Landscape Sensitivity:** Areas along ridgelines, prominent slopes, and vegetated hillsides are highly visible from both within and outside the site, requiring careful consideration in lot layout, building siting, and design.
- **Existing infrastructure:** The existing Melbourne Water pipeline and Glismann Road provide important infrastructure that guides development potential within the GRADP.

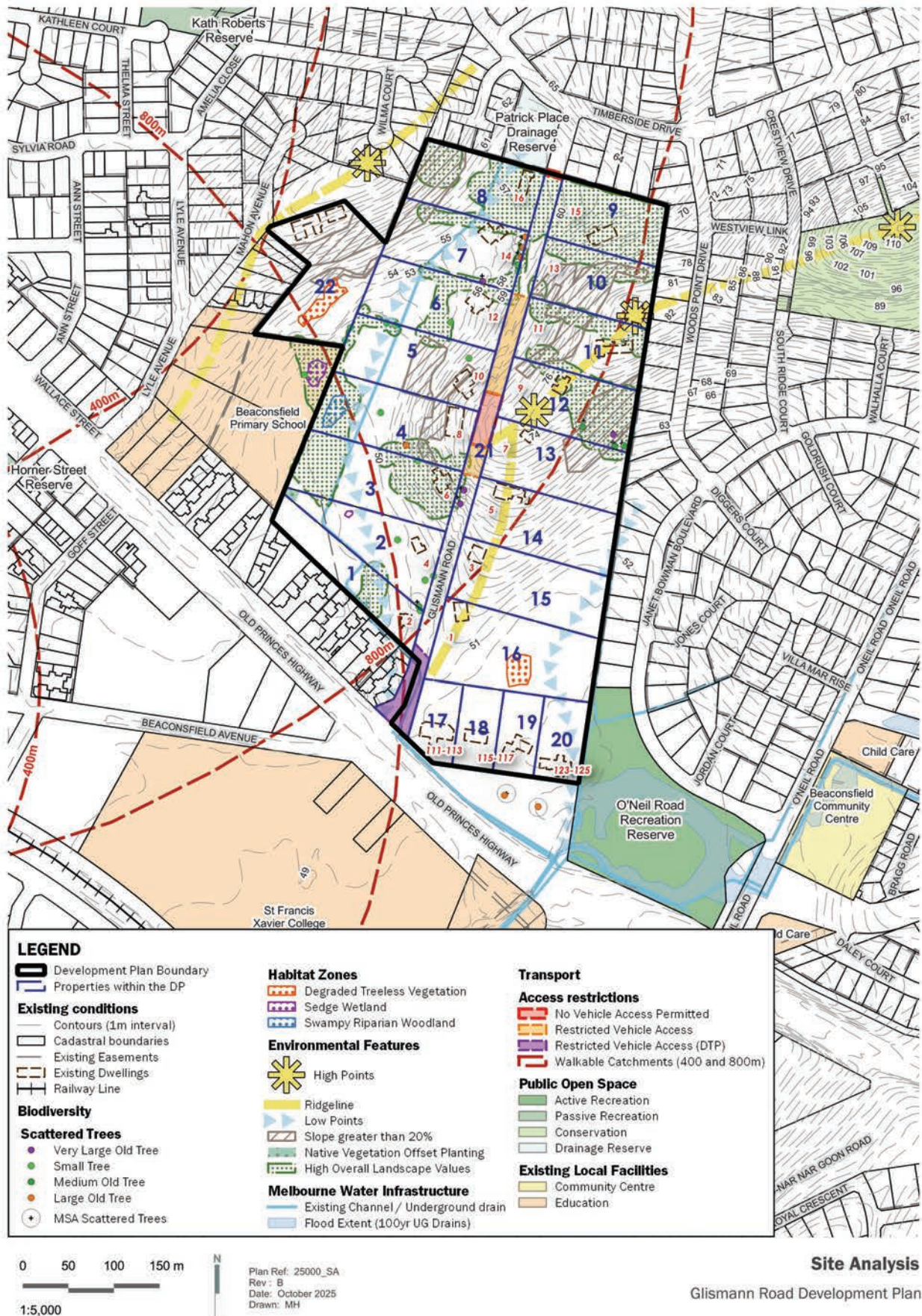


Figure 3 - Site Analysis Plan

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2.2. Existing Topography

Understanding the topography of the GRADP is fundamental to guiding appropriate subdivision and development outcomes. The Pre-Development Slope Plan shown at Figure 4 draws on the detailed analysis provided in the KIP, providing a visual summary of that analysis, highlighting slopes, ridgelines, and valleys.

This information is positioned at the very front of the GRADP to give readers essential context for the key planning issues and guidance in this document, helping to inform building siting, lot layout, infrastructure placement, and the development of design guidelines.

The GRADP is defined by a ridgeline dominating the northeastern portion of the site, extending into higher developed land to the east, while the western part of the GRADP features a ridgeline to the west with a north - south running valley along its base. Significant slopes are located adjacent to these ridgelines, with the valley area between and south of the ridges relatively flat. Slopes along the ridges are predominantly steep to very steep, with some areas above 20% grade classified as 'extremely steep'. Existing dwellings are generally positioned along or just below the ridgeline to maximise outlook and views.

Vegetation is concentrated along the ridges and hillsides, contributing strongly to the area's character and influencing how development can occur on sloping land. Figure 4 illustrates slopes of 10% or greater, which can be generally categorised as follows:

Table 4 - Slope categories

Category	Slope Gradient
Flat	Less than 5%
Moderate Slope	Between 5% and 10%
Steep	Between 10% and 15%
Very Steep	Between 15% and 20%
Extremely Steep	Greater than 20%

These slope categories are referred to throughout the GRADP.

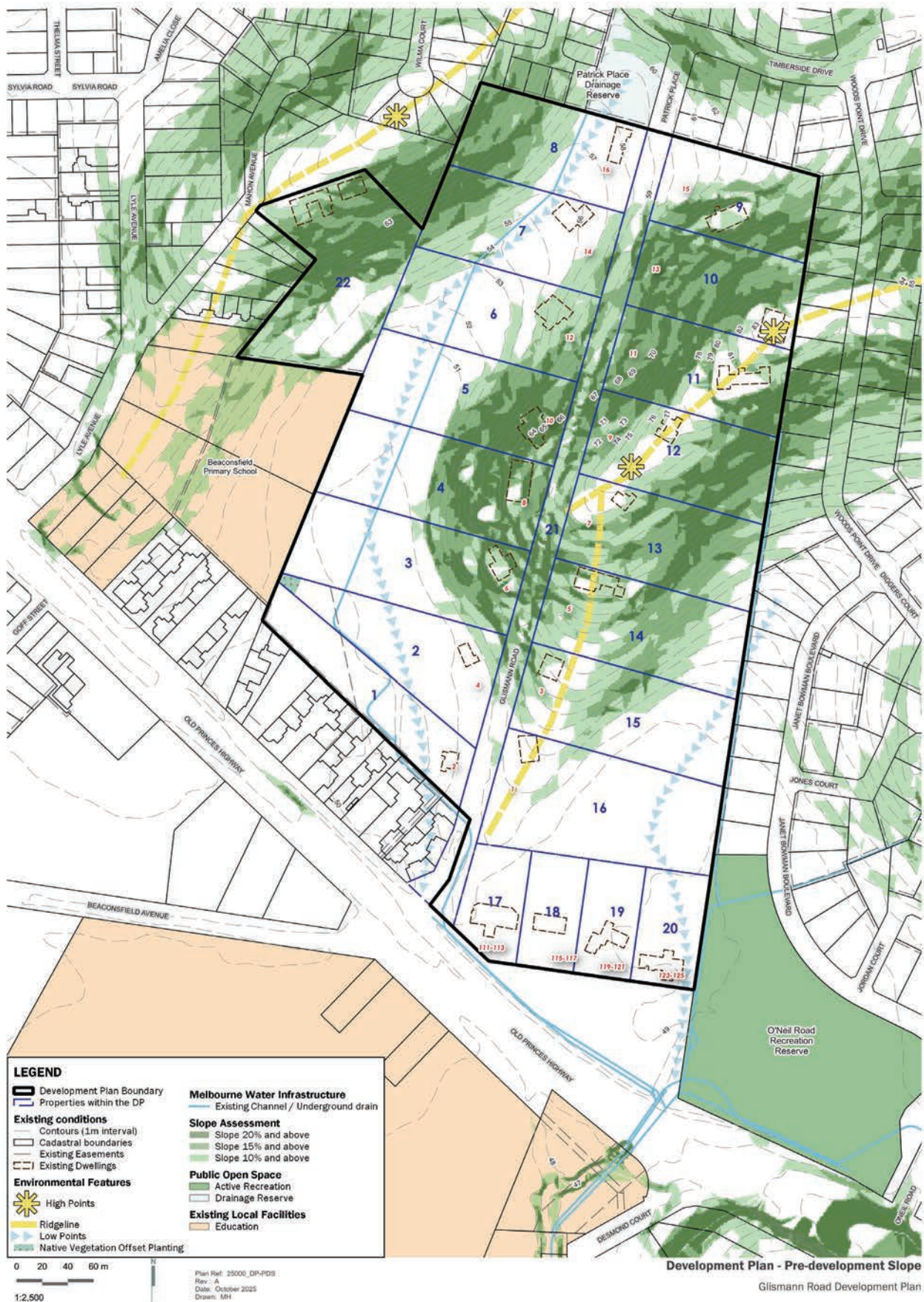


Figure 4 - Pre-Development Slope greater than 10%

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3. Design Response

The Design Response Plan (Figure 5) is reproduced from the KIP and provides a high-level overview of how the opportunities and constraints identified through the KIP and site analysis process are addressed. The plan highlights key design influences that shape the GRADP and should be read in conjunction with the KIP.

The Design Response Plan identifies:

- 1) **Visually sensitive areas** – includes land with slopes assessed as Steep to Extremely Steep (refer to Table 4), land on or adjacent to ridgelines, and areas with high overall landscape values. These areas should be protected from intensive development, and future subdivision and housing designed to respond sensitively to topography, vegetation and landscape character.
- 2) **Interfaces with adjoining land uses** - including the Old Princes Highway, O’Neil Road Recreation Reserve, Beaconsfield Primary School, and the proposed local park. Development should activate adjoining spaces and provide passive surveillance to enhance safety and amenity.
- 3) **Restricted access** - areas along the Glismann Road (DCP Project RD-02) crest, where access limitations are necessary to ensure safe vehicle movements for existing and future dwellings, and where the creation of new access points is restricted.
- 4) **Connectivity** – identifying key connections required to facilitate coordinated subdivision and access between properties. These connections should be designed to respond to topographic constraints, provide alternative connections where feasible, and ensure an integrated local street network.

The Design Response Plan should be read alongside DCP05, relevant DCP project sheets (Glismann Road Development Contributions Plan Project Sheets, Cardinia Shire Council, November 2022), and subsequent sections of the GRADP to understand how infrastructure delivery and topographic constraints have been integrated into the overall design framework.

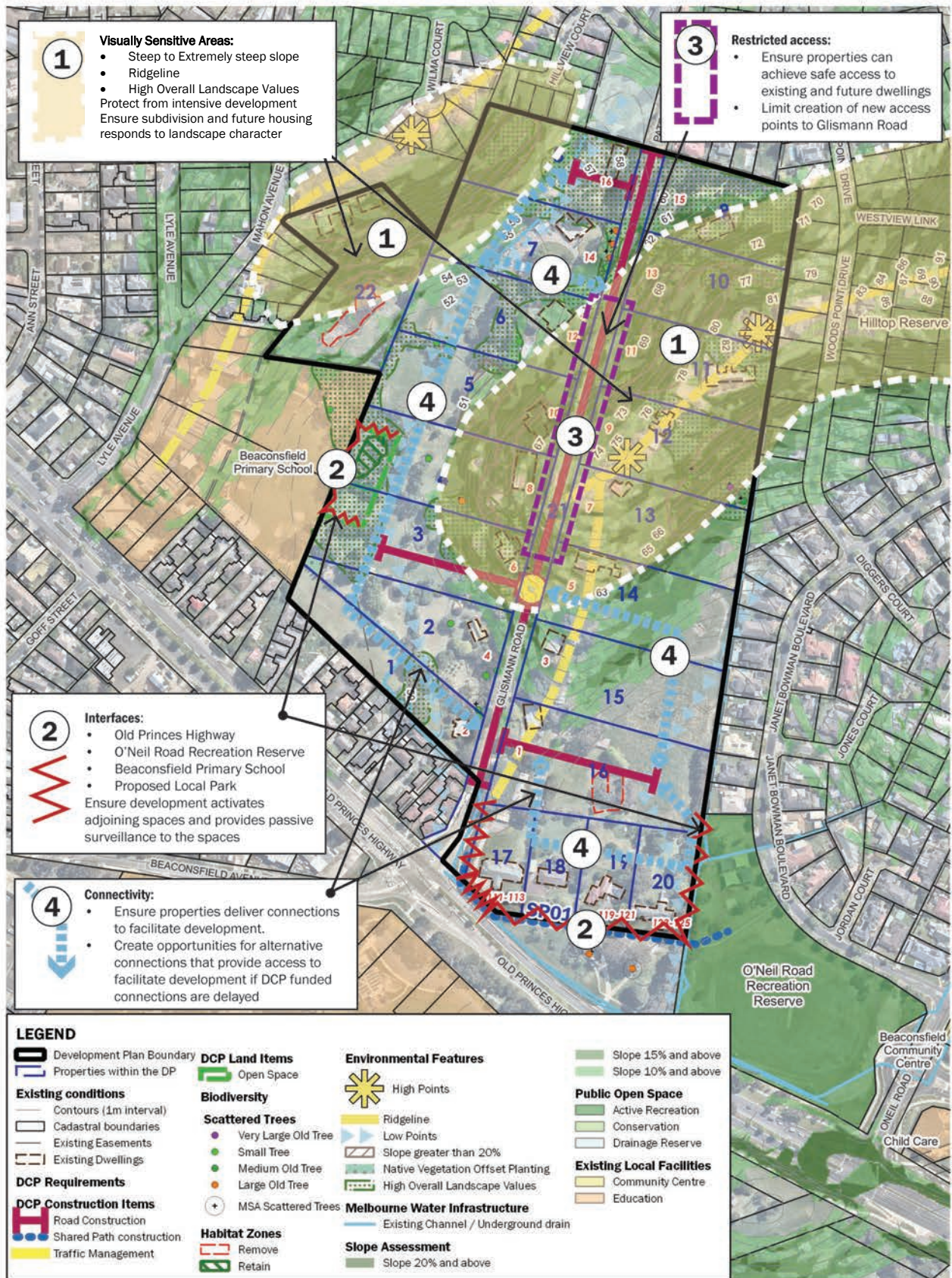


Figure 5 - Design Response Plan

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4. Development Plan

Section 4 introduces the Development Plan for the Glismann Road area, setting out the vision, objectives, and key features that guide the design, layout, and delivery of infrastructure and development outcomes across the area. The GRADP is shown at Figure 6.

4.1. Vision Statement

Glismann Road Area Vision

Glismann Road will become a welcoming and attractive neighbourhood that feels part of Beaconsfield while retaining its unique landscape character.

New homes and streets will be carefully designed and sited to follow the natural shape of the land and protect the area's landscape and views.

The GRADP promotes a co-ordinated approach to development to deliver infrastructure, a diverse range of lot sizes, and attractive streetscapes and open spaces.

Together these elements will create a connected neighbourhood that prioritises tree retention and values site responsive, high quality development outcomes.

4.2. Objectives

The following objectives establish the statutory and design priorities for the GRADP, guiding subdivision, lot layout, infrastructure, and development outcomes to ensure a coordinated, high-quality, and landscape-sensitive precinct:

- Deliver efficient and co-ordinated infrastructure through an overall development framework that aligns subdivision development, infrastructure delivery, and access arrangements across all landholdings.
- Align development with the GRADP's natural topography and landscape features, including the protection and maintenance of the visual prominence of the Glismann Road crest and hillsides, by designing roads, lots, and building envelopes to follow natural contours, minimise earthworks and retain significant vegetation.
- Manage development on pre-development slopes greater than 10% through clear slope management guidelines to ensure sensitive and practical responses to topography.
- Prevent development on areas with pre-development slopes exceeding 20% to protect the natural landform and minimise environmental and visual impacts.
- Promote high quality, site responsive housing through guidelines addressing lot sizes and layout, building envelopes, materials and placement of retaining walls, and siting of buildings and fencing to maintain landscape character and minimise visual impact.
- Encourage housing diversity and affordability by providing a range of lot sizes and dwelling types that respond to community needs and site conditions.
- Prioritise tree retention and strengthen the landscape framework through new canopy planting and additional landscaped areas where appropriate.
- Create a clear, walkable and connected movement network by establishing a legible street hierarchy and providing safe pedestrian and cycle links to existing and future transport connections, taking into account local topography constraints.
- Locate open space to protect natural features and enhance amenity ensuring accessibility, visual connection, and integration with the broader open space network.
- Encourage sustainable and climate sensitive urban design to support long term environmental resilience and energy efficiency.
- Ensure any non-residential uses and developments permitted under the zone provisions (e.g. childcare, medical centres) are appropriately located, designed, and scaled to complement the residential character, landscape setting, and infrastructure capacity of the GRADP area.

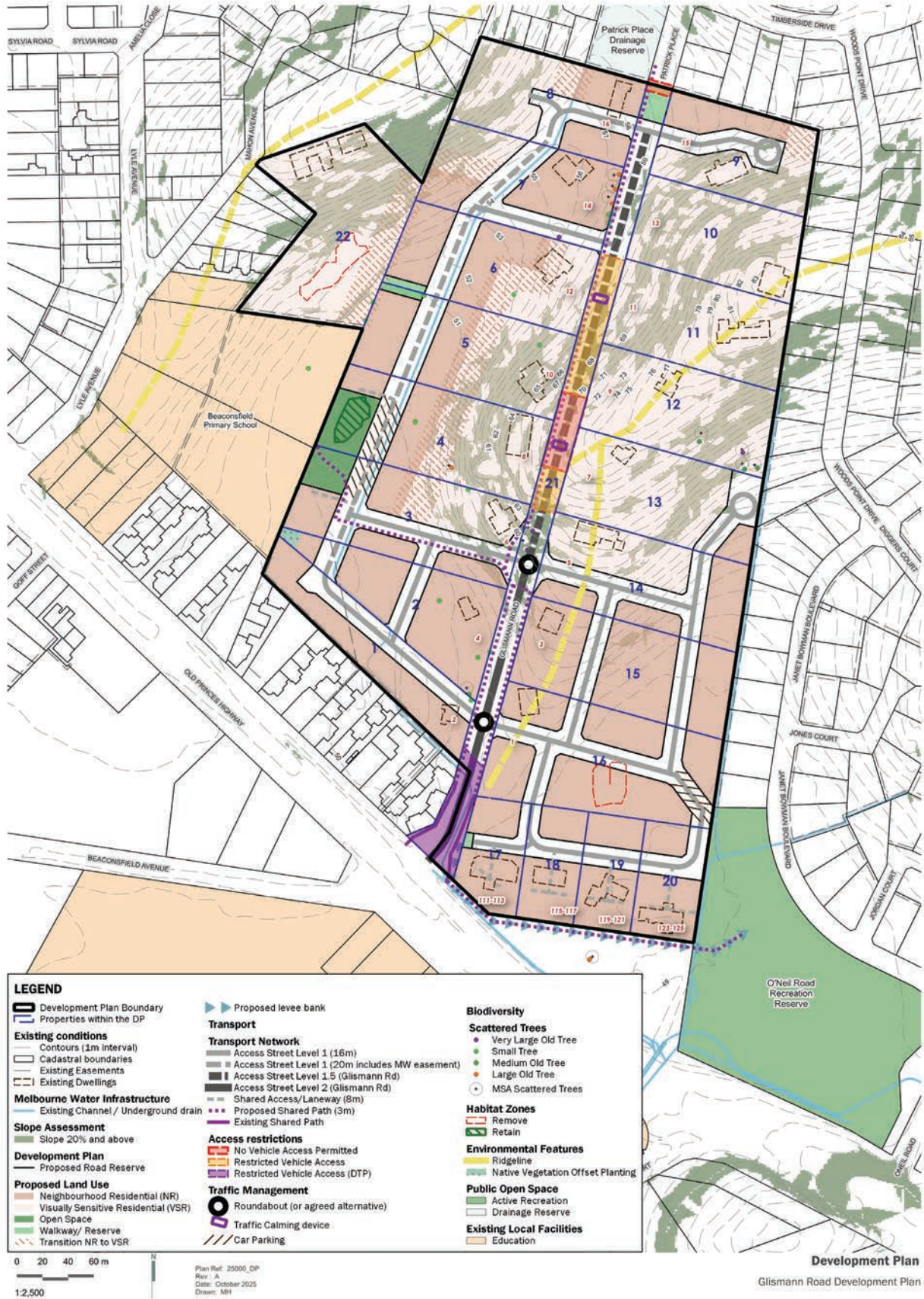


Figure 6 - Development Plan

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4.3. Key Features

The key features of the GRADP are:

- An integrated movement network that coordinates roads, drainage and open space across all parcels, providing a logical framework for equitable and timely infrastructure delivery while connecting with existing shared paths and open space corridors. Due to the complexities of this DP, preliminary discussions with the Responsible Authority in the initial design phase is essential.
- Additional internal road connections that provide alternative options for access and staging, reducing reliance on the timing and delivery of DCPO5 funded items by others.
- ‘Neighbourhood Residential’ and ‘Visually Sensitive Residential’ development areas are identified based on topography and vegetation coverage, ensuring built form responds to site conditions and minimises visual impact. (The application of these areas is indicative and provide a high level framework only. Detailed site analysis is to be undertaken at the planning permit application stage to address topography, vegetation and other site constraints which will ultimately determine the extent of each property within the ‘Neighbourhood Residential’ or ‘Visually Sensitive Residential’ area).
- Roads and lot layouts are aligned with natural contours, with larger lots in steeper areas to minimise earthworks and maintain the natural landscape.
- Slope management guidelines require stepped building forms, controlled retaining wall heights, and vegetation retention to protect the natural landscape with development restricted on slopes exceeding 20%.
- Design guidelines establish consistent parameters for building envelopes, setbacks, materials and fencing and retaining walls to reinforce the landscape character and minimise visual impact.
- Vegetation to be retained is identified, with new canopy plantings to extend the existing landscape character.
- Local open space is provided to complement existing community spaces and create a local focal point, while improved access to O’Neil Road Recreation Reserve enhances visual and physical connectivity.
- Design guidelines promote sustainable outcomes through lot orientation, canopy tree retention and water sensitive urban design measures that support energy efficiency, provide shade, and improve water management.
- A diversity of lot sizes and dwelling types is encouraged to support housing choice and affordability across the precinct.

5. Movement Network

The Movement Network establishes the framework for access, connectivity, and transport integration across the GRADP. It defines how vehicles, pedestrians, and cyclists will move within and beyond the area, ensuring coordinated and efficient connections between individual landholdings and the wider Beaconsfield area.

The design and staging of the network are informed by the KIP, which identified key access constraints, the preferred road hierarchy, and the importance of a coordinated approach to infrastructure delivery.

This section outlines the proposed road hierarchy, access arrangements, and staging principles necessary to deliver a safe, functional, and integrated transport network consistent with the requirements of DPO19 and DCP05. The Movement Network Plan is shown at Figure 7. The overall network is established by DPO19, with several key components also addressed under DCP05. Additional detail on project costs and land valuation methodology is provided in the [Glismann Road Development Contributions Plan Project Sheets \(Cardinia Shire Council, November 2022\)](#).

This Section is structured as follows:

- Section 5.1 – Primary Road Network, as established by DPO19 and DCP05.
- Section 5.2 – Access and Internal Road Options, supporting the Primary Road Network to provide flexibility in development staging.
- Section 5.3 – Other Internal Roads, focusing on local connections within the GRADP.
- Section 5.4 – Cross-Sections, as established by the Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022) and the IDM.
- Section 5.5 – Access Restrictions, as established by DPO19 or where additional DTP consultation and consent is required.
- Section 5.6 – Temporary Access Measures and Arrangements, ensuring safe and reliable access to properties during construction, particularly of Glismann Road.
- Section 5.7 – Shared Path Network, outlining extent, design requirements and funding and delivery responsibilities.
- Section 5.8 – Footpath Network, outlining extent, design requirements and funding and delivery responsibilities.
- Section 5.9 – Access to Public Transport.

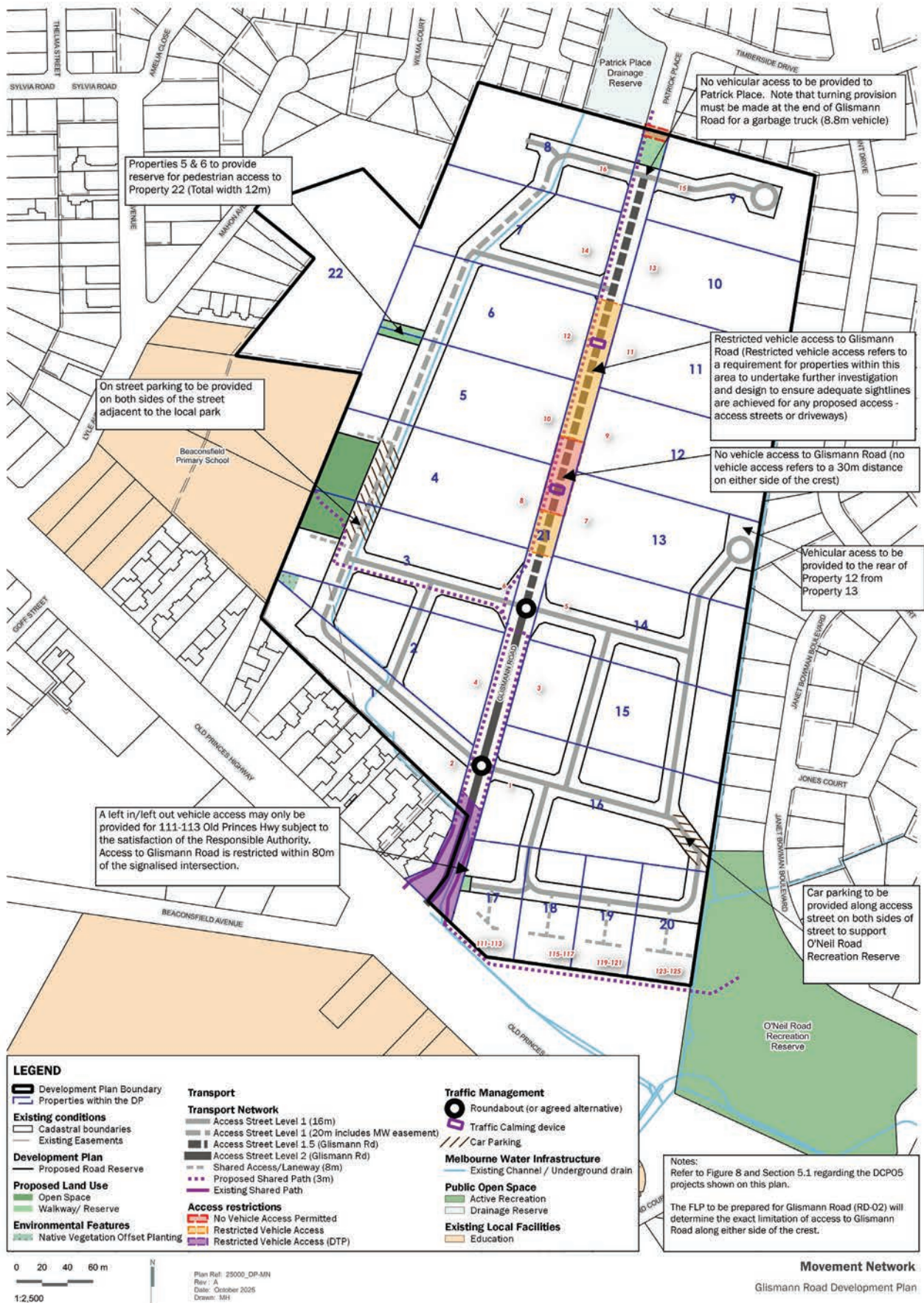


Figure 7 - Transport and Movement Plan

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5.1. Primary Road Network

The primary road network elements of the GRADP included in DCP05 are:

- DCP Project RD-01 (RD-01) – Glismann Road (Access Street Level 2): Old Princes Highway intersection to DCP Project TM-01 (TM-01) (roundabout or alternative treatment to the satisfaction of the Responsible Authority).
- DCP Project RD-02 (RD-02) – Glismann Road (Access Street Level 1.5): TM-01 to truncation of road.
- DCP Project RD-03 (RD-03)– Local access road (Local Access Street Level 1) through Property 3 (6 Glismann Road). Includes a land component DCP Project LA-01 (LA-01).
- DCP Project RD-04 (RD-04)– Local access road (Local Access Street Level 1) through Property 8 (16 Glismann Road). Includes a land component DCP Project LA-02 (LA-02).
- DCP Project RD-05 (RD-05) – Local access road (Local Access Street Level 1) through Property 16 (1 Glismann Road). Includes a land component DCP Project LA-03 (LA-03).
- DCP Project TM-01 (TM-01) – Roundabout at the intersection of Glismann Road and RD-05 (Property 3, 6 Glismann Road). Includes a land component DCP Project LA-04a (LA-04a) and DCP Project LA-04b (LA-04b).

These are shown at **Figure 8**. All or part of the design, construction and land costs for these works are included in DCP05.

5.1.1. Glismann Road (DCP Project RD-01 and DCP Project RD-02)

Glismann Road is the most significant and defining component of this network. Due to substantial differences in topography, Glismann Road is divided into two distinct sections, RD-01 and DCP Project RD-02 which connect at TM-01.

The southern section of Glismann Road (RD-01) has a gentler slope, allowing for staged development with fewer complex design challenges. It is less likely to encounter issues related to Stopping Sight Distances (SSD), or transitions between adjoining landowners.

In contrast, the northern section of Glismann Road (RD-02) presents more significant topographic constraints. Extensive civil and landscape works will be required to achieve compliant grades, adequate sight distances, and effective landscape integration. A key design consideration is the need to lower the existing surface level of Glismann Road by approximately 1.6m at its highest point to meet SSD requirements¹. This adjustment has major implications for how the road is delivered, particularly if lots are developed out of sequence or if some landowners choose not to develop, potentially disrupting the continuity and functionality of the road network.

TM-01, as the connection point between RD-01 and RD-02, plays a critical role in facilitating the transition between the two sections. Its design and construction must accommodate the differing conditions to ensure a functional, cohesive and a safe road network.

¹ Refer to *Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022)* for sections and surface levels.

Due to fragmented land ownership along Glismann Road, there are several unknowns that may affect how and when different sections of the road are delivered. It is likely that some developers may choose to develop out of sequence, which introduces complexity in coordinating infrastructure delivery, particularly for a road with significant topographic variation. As a result, decisions regarding road design, staging, and delivery will ultimately rest with the Responsible Authority, based on the subdivision applications being considered at the time.

To address these complexities, DPO19 requires that all subdivision proposals be accompanied by an Infrastructure Plan. This plan must address the provision, staging, and timing of road works both internal and external to the land. Specifically, the first application to subdivide land with access to Glismann Road (Access Street Level 1.5) (which is RD-02) must include a Functional Layout Plan (FLP) for the entire length of the road, or a length to the satisfaction of the Responsible Authority.

Construction of Glismann Road must occur continuously from the Old Princes Highway, with each subdividing property responsible for delivering its full road frontage. This ensures the road remains a continuous and functional connection as development progresses.

As subdivision applications approach the junction of Glismann Road where RD-01, TM-01, and RD-02 meet, the Responsible Authority may require that the application for subdivision be accompanied by a FLP for the full length of RD-02, TM-01, and a portion of RD-01, or a length of road to the satisfaction of the Authority.

At a minimum, a FLP must be prepared for the first subdivision requiring access to RD-02. This FLP must include TM-01 (refer to Table 10 for further information). The FLP must be prepared and approved for the full length of RD-02, or a length as agreed by the Responsible Authority. The FLP must also address Property 3 and Property 14, which require TM-01 to provide site access, unless otherwise agreed by the Responsible Authority.

The following tables provide a summary of the extent, construction triggers, design requirements, and funding and delivery arrangements for each element of the primary road network. For further information refer to the DCP the [Glismann Road Development Contributions Plan Project Sheets \(Cardinia Shire Council, November 2022\)](#), and the [Glismann Road Traffic Impact Assessment Report, TrafficWorks \(1 May 2022\)](#). Due to the complexities of this DP, preliminary discussions with the Responsible Authority in the initial design phase is essential.



Figure 8 - Movement Network - DCP05 Transport items

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Table 5 - RD-01 – Glismann Road – Access Street Level 2 (cross section at Figure 11)

Extent	<ul style="list-style-type: none"> From the Old Princes Highway intersection to TM-01 within the existing road reservation. No land acquisition is required or included in DCPO5.
Construction Trigger	<ul style="list-style-type: none"> The first subdivision requiring access to RD-01 must construct RD-01. Construction must be continuous from the Old Princes Highway intersection to the full extent of the subdividing property’s RD-01 frontage. Any following subdivision must deliver the continuous construction of RD-01.
Design Requirements	<ul style="list-style-type: none"> If the subdivision requires the provision of TM-01, the first subdivision in RD-01 must prepare a FLP for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority. An allowance for a FLP for RD-02 is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details.
Funding & Delivery	<ul style="list-style-type: none"> DCPO5 includes the design and construction of this project as outlined in the DCP and the Glismann Road Development Contributions Plan Project Sheets (Cardinia Shire Council, November 2022)
On-street Parking	<ul style="list-style-type: none"> Indented 2.3m wide parking lanes along its length.
Shared Paths & Footpaths	<ul style="list-style-type: none"> 3m wide shared path on western side. 1.5m wide footpath on eastern side.

Table 6 - RD-02 – Glismann Road – Access Street Level 1.5 (cross section at Figure 12)

Extent	<ul style="list-style-type: none"> From TM-01 to the truncation of Glismann Road within the existing road reservation. No land acquisition is required or included in DCPO5.
Construction Trigger	<ul style="list-style-type: none"> The first subdivision requiring access to RD-02 must construct RD-02 to the full extent of the subdividing property’s RD-02 frontage. RD-01 and TM-01 must also be constructed if these items have not been constructed.
Design Requirements	<ul style="list-style-type: none"> A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details. Construction requires lowering the existing surface level of Glismann Road by approximately 1.6m at its highest point to achieve safe Stopping Sight Distances (SSD). No vehicle access from individual properties, including roads, driveways and parking, is permitted at the crest of the hill and 30m either side of the crest (‘No Vehicle Access Permitted’ area). Refer to Section 5.5 for further details. The design speed through the crest is 40km/h. Traffic calming devices (e.g. slow points or horizontal deflection devices) must be implemented on each approach to the crest, with specific treatment to be implemented subject to detailed design and to the satisfaction of the Responsible Authority.

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	<ul style="list-style-type: none"> • Driveways only may be permitted within the RD-02 'Restricted Vehicle Access' area subject to a sight distance assessment. No road connections to Glismann Road are permitted within this area. Refer to Section 5.5 for further details. • The truncation of the road must provide the ability for a waste vehicle to turn in a forward direction by provision of a court bowl with a 10.5m radius or other alternative solution approved by the Responsible Authority².
Funding & Delivery	<ul style="list-style-type: none"> • Unlike RD-01, DCPO5 includes partial funding for RD-02 and only includes items associated with design (FLP), siteworks, earthworks and retaining wall components, and includes a 30% contingency³. • The elements of the DCP project are restricted to an allowance for excavation, fill, set out, retaining walls as well as the cost to prepare the Functional Layout Plan (FLP). All other construction costs (e.g. road pavement, concrete works, drainage, traffic calming devices, landscaping, street lighting, miscellaneous items, and delivery) are not covered by the DCP and are to be met by the respective developer(s).
On-street Parking	<ul style="list-style-type: none"> • No on-street parking is permitted.
Shared Paths & Footpaths	<ul style="list-style-type: none"> • 3m wide shared path on western side. • No footpath is included in cross-section.

Table 7 - RD-03 – Local Access Street Level 1 (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> • Road construction for 120m of road within a 16m wide road reserve, located within Property 3. • DCP project LA-01 is for the land cost for RD-03 (16m x 120m = 1,920sqm)⁴.
Construction Trigger	<ul style="list-style-type: none"> • To be constructed with any subdivision of Property 3. • If not already constructed, the northern roundabout (TM-01) and Glismann Road (RD-01) must be concurrently constructed. Refer to Section 5.1.1 for further details.
Design Requirements	<ul style="list-style-type: none"> • Construct to Infrastructure Design Manual (IDM) standards in accordance with the typical cross-section shown at Section 5.4. • A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. • An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details. • The intersection of RD-03 with RD-02 must be designed to provide TM-01. TM-01 must be designed to provide a fourth-leg to Property 14. Refer to Table 10 and Figure 10 for further details.

² Refer to Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022).

³ Refer to the RD02 project sheet in the Glismann Road Development Contributions Plan (Urban Enterprise, amended December 2023); Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022).

⁴ Refer to the RD02 project sheet in the Glismann Road Development Contributions Plan (Urban Enterprise, amended December 2023); Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022).

	<ul style="list-style-type: none"> The indicative roundabout location shown in Figure 10 is generally in accordance with that shown in Figure 1 of DPO19 and Figure 2 of the DCP document. There is no allowance in the DCP for the splays within Property 3 required for TM-01. Provision must be made for a local connection to Property 2 and Property 4.
Funding & Delivery	<ul style="list-style-type: none"> Land is to be set aside and vested in the Responsible Authority with any subdivision of Property 3. DCPO5 provides for construction (RD-03) and associated land costs (LA-01).
On-street Parking	<ul style="list-style-type: none"> Indented 2.3m wide car parking lanes along its length. On-street parking to be provided on both sides of the carriageway adjacent to the local open space (LA-05).
Shared Paths & Footpaths	<ul style="list-style-type: none"> 3m wide shared path on one side connecting to local open space. 1.5m wide footpath on other side.

Table 8 – RD-04 – Local Access Street Level 1 (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> Road construction of 50m within a 16m wide road reserve, located within Property 8. DCP project LA-02 is for the land cost for RD-04 (16m x 50m = 800sqm)⁵.
Construction Trigger	<ul style="list-style-type: none"> To be constructed with any subdivision of Property 8. If not already constructed, Glismann Road (RD-01 and RD-02) and TM-01 must be concurrently constructed. Refer to Section 5.1.1 for further details.
Design Requirements	<ul style="list-style-type: none"> Construct to Infrastructure Design Manual (IDM) standards in accordance with the typical cross-section shown at Section 5.4. A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details. Provision must be made for a local connection to Property 7 as part of the western loop road. This is outlined in further detail in Section 5.3.
Funding & Delivery	<ul style="list-style-type: none"> Land is to be set aside and vested in the Responsible Authority with any subdivision of Property 8. DCPO5 provides for construction (RD-04) and associated land costs (LA-02).
On-street Parking	<ul style="list-style-type: none"> Indented 2.3m wide parking lanes along its length.
Footpaths	<ul style="list-style-type: none"> 1.5m wide footpath on both sides of road.

⁵ Refer to #3.

Table 9 – RD-05 - Local Access Street Level 1 (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> • Road construction of 149m within a 16m wide road reserve, located within Property 16. • DCP project LA-03 is for the land cost for RD-05 (16m x 149m = 2,384sqm)⁶.
Construction Trigger	<ul style="list-style-type: none"> • To be constructed with any subdivision of Property 16. • If not already constructed, Glismann Road (RD-01) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage.
Design Requirements	<ul style="list-style-type: none"> • Construct to Infrastructure Design Manual (IDM) standards in accordance with the typical cross-section shown at Section 5.4. • Provision must be made for local connections to Property 15, 17, 18, 19 and 20. This is discussed further in Section 6. • The intersection with RD-01 must be designed to accommodate a future four-way roundabout with RD-05 to manage access to Property 1 and Property 16. The construction or additional land required beyond the Glismann Road reserve is not a DCP project. Further information is provided in • Table 11 regarding this requirement. • If the subdivision requires the provision of TM-01, the first subdivision in RD-01 must prepare a FLP for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details.
Funding & Delivery	<ul style="list-style-type: none"> • Land is to be set aside and vested in the Responsible Authority with any subdivision of Property 16. • DCPO5 provides for construction (RD-05) and associated land costs (LA-03). • The roundabout must be constructed when access to Property 1 is constructed unless otherwise agreed by the Responsible Authority. • All design, construction and land costs for the roundabout are to be met by whichever property proceeds first.
On-street Parking	<ul style="list-style-type: none"> • Indented 2.3m wide car parking lanes along its length. • On-street parking to be provided on both sides of the carriageway adjacent to O’Neil Road Recreation Reserve.
Footpaths	<ul style="list-style-type: none"> • 1.5m wide footpath on both sides of road.

⁶ Refer to the RD02 project sheet in the Glismann Road Development Contributions Plan (Urban Enterprise, amended December 2023); Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022).

Table 10 - TM-01 – Roundabout (or alternative treatment to the satisfaction of the Responsible Authority) (cross-section at Figure 10 for concept plan)

<p>Extent</p>	<ul style="list-style-type: none"> • The preferred form of TM-01 is a roundabout, however an alternative intersection treatment may be considered and approved by the Responsible Authority subject to detailed design and completion of required assessments and the outcome of the FLP. • TM-01 is located at the intersection of Glismann Road (between RD-01 and RD-02) and Access Street Level 1 (RD-03).
<p>Construction Trigger</p>	<ul style="list-style-type: none"> • To be constructed: <ul style="list-style-type: none"> • By any development beyond the northern extent of RD-01 (including Property 3 or Property 14), or • If access is required for the development of Property 17 (111-113 Old Princes Highway) from Glismann Road (RD-01) and a roundabout has not been constructed to provide access to Property 1 and Property 16. Further information is provided in <ul style="list-style-type: none"> • Table 9 and • Table 11 regarding this requirement.
<p>Design Requirements</p>	<ul style="list-style-type: none"> • A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to the road (RD-02) or TM-01. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details. • TM-01 must be designed to provide access to Properties 3 and 14. Refer to Figure 10 for indicative concept plan for a roundabout solution. The location shown in Figure 10 is generally in accordance with that shown in Figure 1 of DPO19 and Figure 2 of the DCP document. • There will be no public acquisition or vesting of land in Property 3 to facilitate the design of TM-01. This constraint must be considered in the design of TM-01, particularly if Property 3 does not intend to develop at the time TM-01 is scheduled for delivery.
<p>Funding & Delivery</p>	<ul style="list-style-type: none"> • Land to be set aside and vested in Council in Properties 14 and 15 if TM-01 is required to be constructed (refer to Construction Trigger). • DCPO5 covers the construction of TM-01 and the associated limited land costs (LA-04a and LA-04b). • In accordance with DCP requirements, any additional construction costs or land needed from Properties 14 and 15 to deliver TM-01, beyond what is allocated in the DCP, will be the responsibility of the developer. This is likely to be the case if Property 3 is not being developed concurrently.
<p>Shared Paths & Footpaths</p>	<ul style="list-style-type: none"> • 3m wide shared path on western side. • 1.5m wide footpath on eastern side.

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5.2. Access and Internal Road Options

To support the primary road network and ensure flexible, coordinated development across the GRADP, additional access options and internal roads are provided beyond that shown in Figure 1 of DPO19 and DCPO5 funded works shown in Figure 2 of the DCP document. These roads facilitate development, improve connectivity between parcels, and enable integrated infrastructure delivery independent of DCPO5-funded works. These are:

- Additional access options onto Glismann Road via Property 1, Property 7, and Property 14. The design, construction and land costs for these additional access roads, or the other internal road options shown, are not included in DCPO5 and are to be met by the respective developer(s). They are developer works.
- An internal road network that enables coordinated and integrated development connections across multiple land parcels. This network allows movement planning to occur holistically rather than in isolation, supporting seamless access and infrastructure delivery.

Together, these additional connections provide alternative options for access and staging, reducing reliance on the timing and delivery of DCPO5 funded items by other parties.

To facilitate access to Property 1 and Property 16, a southern roundabout is proposed at Glismann Road (RD-01) (refer to Figure 9 for concept plan). This roundabout is not a DCP requirement, and alternative solutions may be considered subject to the approval of the Responsible Authority and DTP given its proximity to the signalised intersection at Glismann Road and Old Princes Highway.

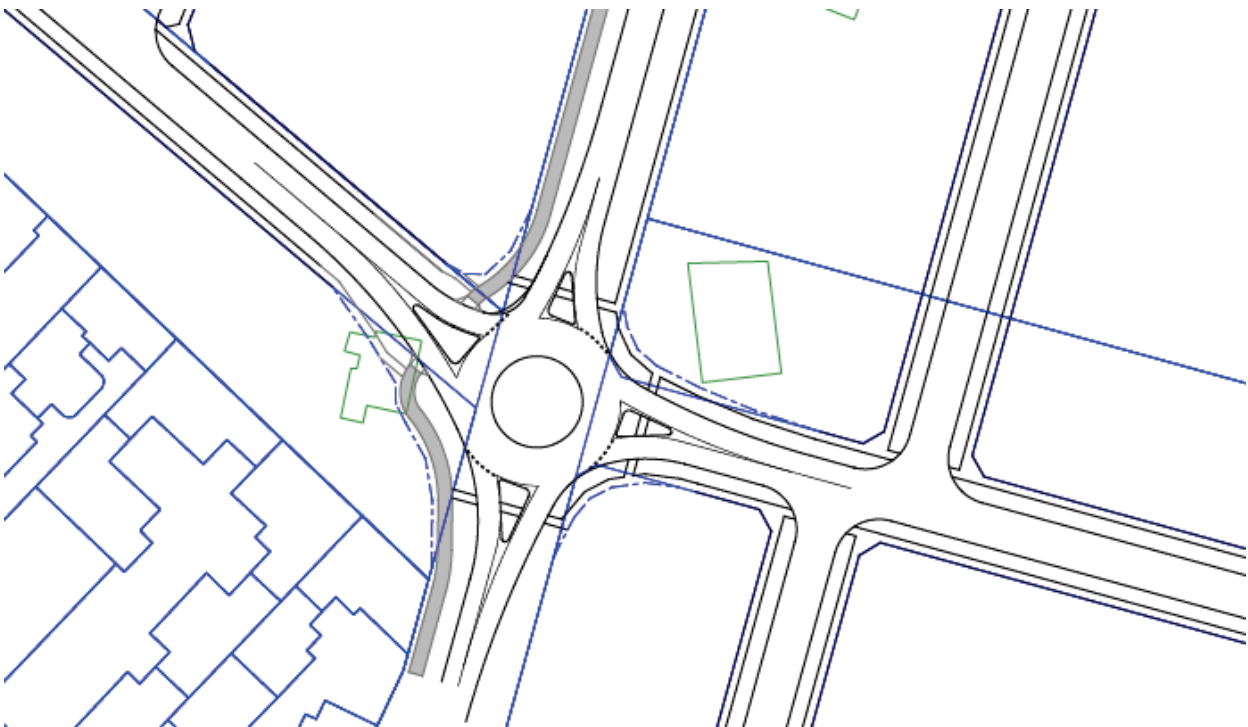


Figure 9 - Concept Plan for southern roundabout to provide access from Glismann Road to Property 1 and Property 16

The following tables provide a summary of the extent, construction triggers, design requirements, and funding and delivery arrangements for each of these additional access and internal roads.

Table 11 - Property 1 (2 Glismann Road) – Local Access Street Level 1 (16 m) (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> Local Access Street (16 m wide) along the northern boundary of Property 1, vested in the Responsible Authority.
Construction Trigger	<ul style="list-style-type: none"> To be constructed with the subdivision or development of Property 1. If not already constructed, Glismann Road (RD-01 DCP project) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. Refer to Section 5.1.1 for further details.
Design Requirements	<ul style="list-style-type: none"> Construct to Infrastructure Design Manual (IDM) standards in accordance with the typical cross-section shown at Section 5.4. Provision must be made for two connections to Property 2: <ul style="list-style-type: none"> 16 m wide local access street approximately mid-point along the shared boundary, and 20 m wide local access street forming part of the western loop road connection. A temporary court bowl must be provided until the future connection is required to Property 2. The intersection with RD-01 must be designed to accommodate a future four-way roundabout with RD-05.
Funding & Delivery	<ul style="list-style-type: none"> Developer funded and constructed. The roundabout is to be constructed when RD-05 is constructed, unless otherwise agreed by the Responsible Authority. All design, construction and land costs for the roundabout are to be met by whichever property proceeds first.
On-street Parking	<ul style="list-style-type: none"> To be determined through detailed design. Likely one or both sides of the carriageway.
Footpaths	<ul style="list-style-type: none"> 1.5m wide footpath on both sides of road.

Table 12 - Property 7 (14 Glismann Road) – Local Access Street Level 1 (16 m) (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> Local Access Street (16 m wide) along the southern boundary of Property 7, vested in the Responsible Authority.
Construction Trigger	<ul style="list-style-type: none"> To be constructed with the subdivision of Property 7. If not already constructed, Glismann Road (RD-01 and RD-02) and TM-01 must be concurrently constructed. Refer to Section 5.1.1 for further details.

Design Requirements	<ul style="list-style-type: none"> • A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details. • Construct to IDM standards in accordance with the typical cross-section at Section 5.4. • The road terminates at the western loop road connection (20m wide Local Access Street). A temporary court bowl will be required until connection into Property 6 and 8 is complete. • The intersection with Glismann Road must be located outside the 'Restricted Vehicle Access' area.
Funding & Delivery	<ul style="list-style-type: none"> • Developer funded and constructed.
On-street Parking	<ul style="list-style-type: none"> • To be determined through detailed design. Likely one or both sides of the carriageway.
Footpaths	<ul style="list-style-type: none"> • 1.5m wide footpath on both sides of road.

Table 13 - Property 14 (5 Glismann Road) – Local Access Street Level 1 (16 m) (cross-section at Figure 14)

Extent	<ul style="list-style-type: none"> • Local Access Street (16 m wide) through Property 14 to provide a fourth leg to TM-01, vested in the Responsible Authority. • Refer to Section 5.1 for Primary Road Network requirements and specific construction and timing requirements for TM-01. • DCP Projects LA-04a and LA-04b fund the splays for TM-01 on the eastern side of Glismann Road. LA-04a allocates 115sqm for Property 15 (3 Glismann Road) and LA-04b allocates 366sqm for Property 14 (5 Glismann Road). There is no allowance in the DCP for splays, should they be required, on the western side of Glismann Road to accommodate TM-01 or should they exceed the requirements allocated in the DCP in LA-04a and LA-04b.
Construction Trigger	<ul style="list-style-type: none"> • To be constructed with the subdivision of Property 14. • If not already constructed, the northern roundabout (TM-01) and Glismann Road (RD-01) must be concurrently constructed. Refer to Section 5.1.1 for further details.

Design Requirements	<ul style="list-style-type: none"> • A FLP must be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. An allowance for a FLP is included in DCP05. The FLP must include TM-01. Refer to Section 5.1.1 for further details. • Construct to IDM standards in accordance with the typical cross-section at Section 5.4. • Provision must be made for: <ul style="list-style-type: none"> • Two connections to Property 15: <ul style="list-style-type: none"> • 16 m wide local access street approximately mid-point, and • 16 m wide local access street forming part of the eastern road network. • One 16 m wide local access street connection to Property 13 as part of the eastern road network. • A temporary court bowl must be provided until the future connection to Property 13 or 15 is required. • The intersection with RD-02 must be designed to accommodate TM-01 and include provision for connection to RD0-3. Refer to Section 5.1 for Primary Road Network requirements and specific construction and timing requirements for TM-01. • TM-01 must be designed to provide access to Properties 3 and 14. Refer to Figure 10 for indicative concept plan for a roundabout solution. The location shown in Figure 10 is generally in accordance with that shown in Figure 1 of DPO19 and Figure 2 of the DCP document.
Funding & Delivery	<ul style="list-style-type: none"> • Connections of this Local Access Street to RD-01 and TM-01 are DCP items. Limited allowances in the DCP are provided for RD-02. All other road construction and any additional land costs beyond DCP allowances to be met by the developer.
On-street Parking	<ul style="list-style-type: none"> • To be determined through detailed design. Likely one or both sides of the carriageway.
Footpaths	<ul style="list-style-type: none"> • 1.5m wide footpath on both sides of road.

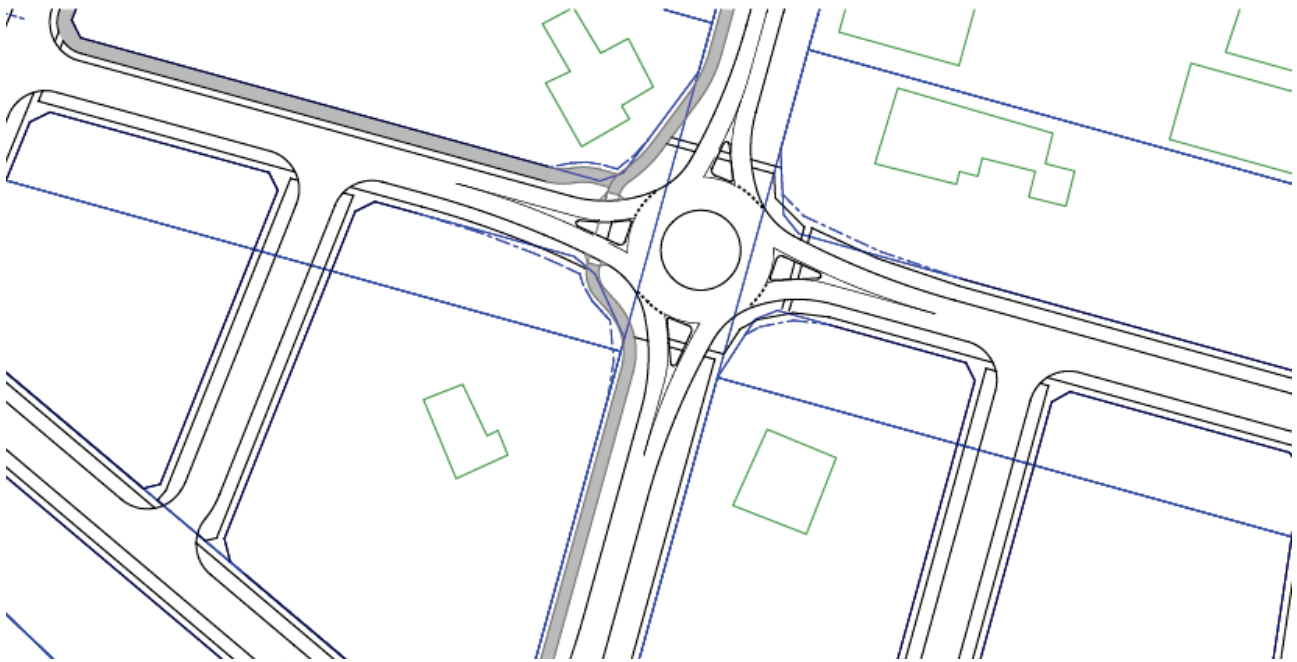


Figure 10 - TM-01 concept design including additional eastern leg to Property 14

Table 14 - Property 17 (111 – 113 Glismann Road) – left in/out access

Extent	<ul style="list-style-type: none"> • Driveway access in accordance with DPO19. • No through road will be permitted to Glismann Road for Property 18, 19 or Property 20 via Property 17.
Construction Trigger	<ul style="list-style-type: none"> • To be constructed with the development of Property 17.
Design Requirements	<ul style="list-style-type: none"> • Provision of an all movement intersection in this location may be possible subject to agreement from DTP and the Responsible Authority. Agreement from these Authorities is also required to confirm the location of direct access restricted to left in/out operation only. • Construction of a roundabout, shown at the intersection of Property 1 and Property 16, may provide alternative access solutions for Property 17. This would need to be discussed with relevant Property holders to facilitate the roundabout construction. • Further design and access considerations for Property 17, 18, 19 and 20 is provided in Section 6. • If the subdivision requires the provision of TM-01, the first subdivision in RD-01 must prepare a FLP for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority. An allowance for a FLP is included in DCPO5. The FLP must include TM-01. Refer to Section 5.1.1 for further details.
Funding & Delivery	<ul style="list-style-type: none"> • Developer funded and constructed. • If a roundabout is constructed to facilitate alternative access to Property 17, all associated costs will be borne by Property 17, unless otherwise agreed by the Responsible Authority.

5.3. Other internal roads

The following tables provide a summary of the extent, construction triggers, design requirements, and funding and delivery arrangements for all other roads.

Table 15 - Western Loop Road – Local Access Street Level 1 (cross-section at Figure 13)

Extent	<ul style="list-style-type: none"> The Western Loop Road extends adjacent to the western property boundary, forming a continuous street network between Property 1 and Property 8. This additional north-south link enhances connectivity across the Development Plan area, which is one of the key objectives of the DP. The road also facilitates convenient access to the local open space and provides a dedicated pedestrian connection to Property 22 (11 Mahon Avenue), supporting walkability and integrated access throughout the precinct. The road alignment is also adjacent to the existing Melbourne Water pipe.
Construction Trigger	<ul style="list-style-type: none"> To be constructed as part of individual Property subdivision.
Design Requirements	<ul style="list-style-type: none"> Construct to IDM standards in accordance with the typical cross-section at Section 5.4. 20 m wide Local Access Street Level 1 that includes a 4m wide Melbourne Water easement on its eastern side (refer to Figure 13 - Western Loop Road cross-section – Access Street Level 1 (20m) with Melbourne Water pipeline). A reserve must be provided from Property 5 and 6 to Property 22 as specified in DPO19 to facilitate a pedestrian connection. The location, design and construction of the pedestrian connection must be to the satisfaction of the Responsible Authority and respond to site topography and facilitate accessible grades. This is not a DCP funded item. Provision for temporary court bowls must be made if not directly connecting to an existing portion of road.
Funding & Delivery	<ul style="list-style-type: none"> All road construction costs are to be met by the developer.
On-street Parking	<ul style="list-style-type: none"> To be determined through detailed design. Likely one or both sides of the carriageway. On-street parking to be provided on both sides of the carriageway adjacent to the local open space (DCP Project LA-05a (LA-05a) and DCP Project LA-05b (LA-05b)).
Shared Paths & Footpaths	<ul style="list-style-type: none"> 3m wide shared path connection between RD-03 and the local open space only. 1.5m wide footpath on both sides of road (except for dot point above).

Table 16 - Other Roads (cross section at Figure 14)

Extent	<ul style="list-style-type: none"> As required.
Construction Trigger	<ul style="list-style-type: none"> To be constructed as part of individual Property subdivision.
Design Requirements	<ul style="list-style-type: none"> 16 m wide local road street, construct to IDM standards in accordance with the typical cross-section at Section 5.4. Provision for temporary court bowls must be made if not directly connecting to an existing portion of road.
Funding & Delivery	<ul style="list-style-type: none"> All road construction costs are to be met by the developer.
On-street Parking	<ul style="list-style-type: none"> To be determined through detailed design. Likely one or both sides of the carriageway. On-street parking to be provided on both sides of the carriageway adjacent to O’Neil Road Recreation Reserve, requiring a shuttle flow to allow two-way traffic movements.
Footpaths	<ul style="list-style-type: none"> 1.5m wide footpath on both sides of road.

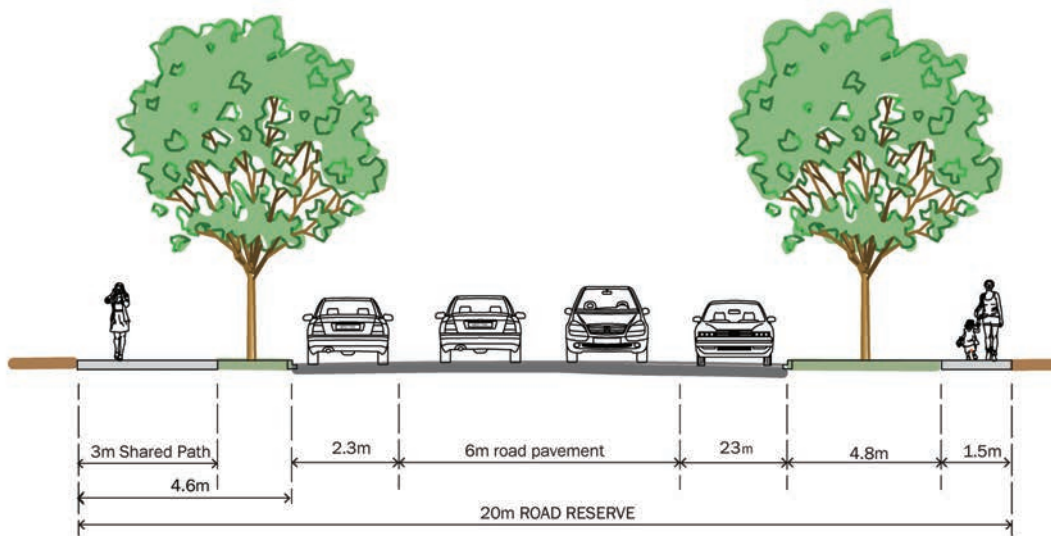
Table 17 - Shared Access / Laneway (cross section at Figure 15)

Extent	<ul style="list-style-type: none"> As required.
Construction Trigger	<ul style="list-style-type: none"> To be constructed as part of individual Property subdivision.
Design Requirements	<ul style="list-style-type: none"> 8 m wide local road access, see cross sections included at Section 5.4.
Funding & Delivery	<ul style="list-style-type: none"> All road construction costs are to be met by the developer.
On-street Parking	<ul style="list-style-type: none"> Not permitted.
Footpaths	<ul style="list-style-type: none"> Not required provided traffic volumes are less than 300 vehicles per day.

5.4. Cross Sections

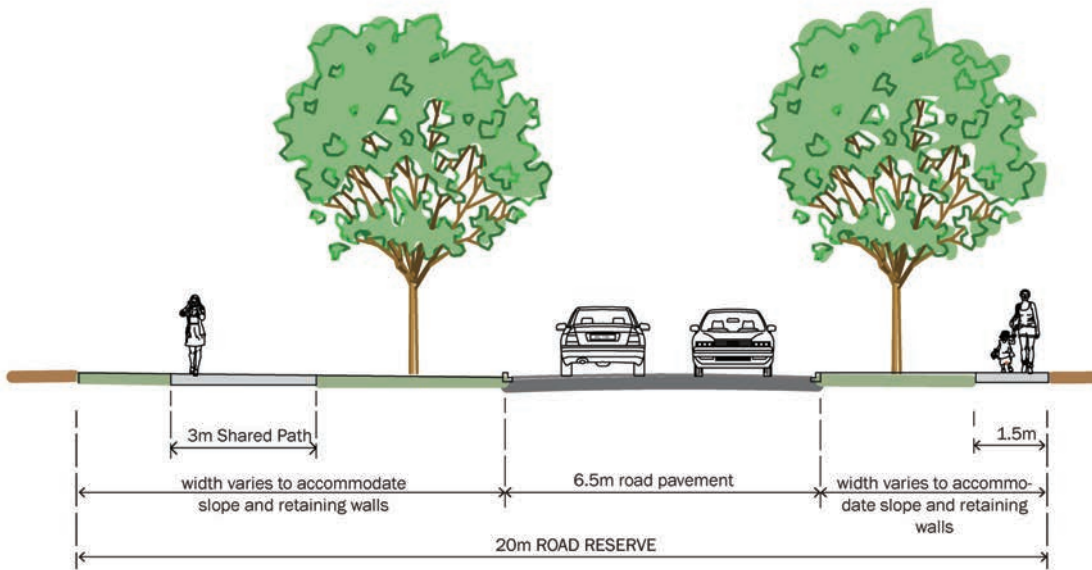
Cross sections for each road type are provided below, to demonstrate the standard of construction required for each type. All streets should be designed in accordance with the below cross sections, unless with the approval of the Responsible Authority. The following cross sections are based on advice provided in the Traffic Impact Assessment Report referenced in DPO19.

The FLP to be prepared for Glismann Road (RD-02) will provide further detail in relation to the final cross section for RD-02 which will include details regarding retaining walls and cut and fill.



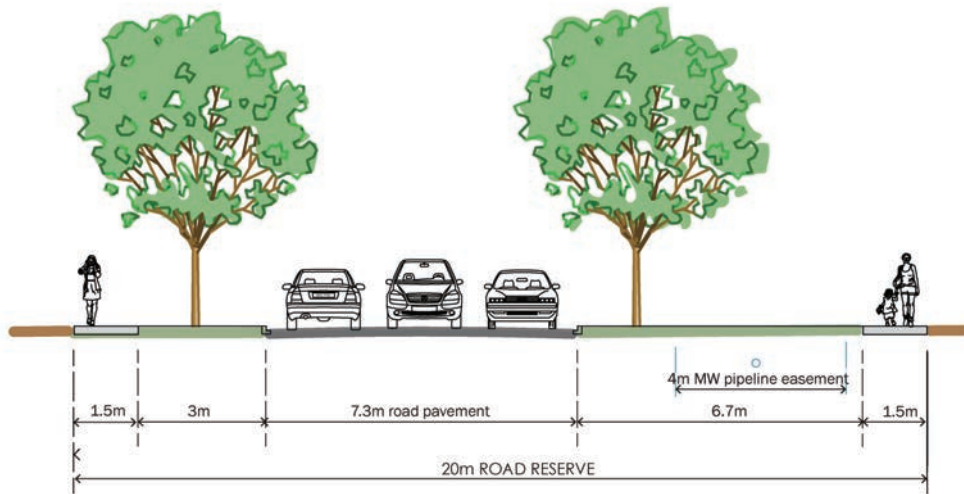
Access Street Level 2 (20m - Glismann Road)

Figure 11 - Glismann Road RD-01 cross-section - Access Street Level 2 (20m)



Access Street Level 1.5 (20m - Glismann Road)

Figure 12 - Glismann Road RD-02 cross-section – Access Street Level 1.5 (20m)

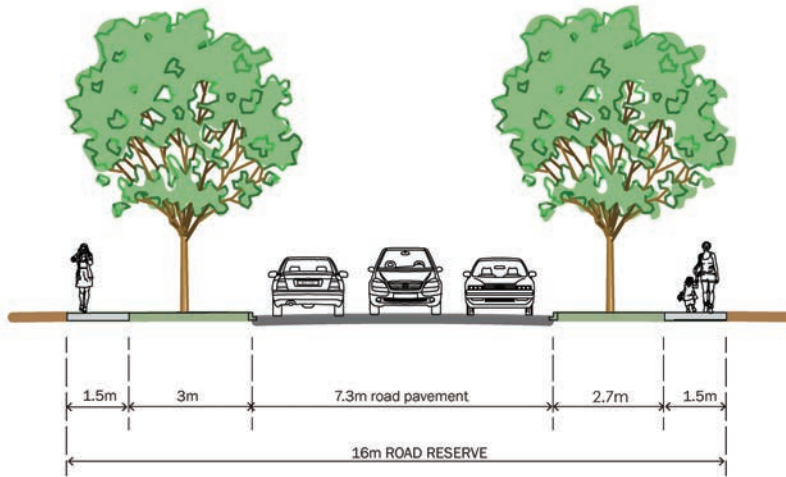


Access Street Level 1 (20m with MW pipeline)

Note: Replace footpath on west side of street reserve with shared path along local park interface from Local Park to connect to RD-03

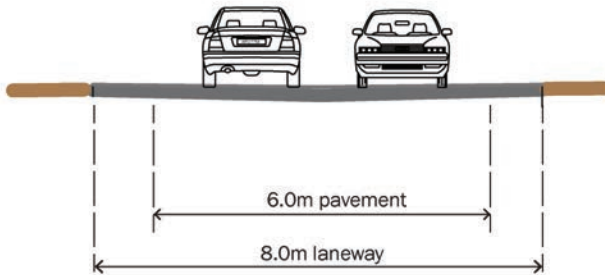
Figure 13 - Western Loop Road cross-section – Access Street Level 1 (20m) with Melbourne Water pipeline

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Access Street Level 1 (16m)

Figure 14 - Access Street Level 1 (16m) cross-section



Shared Access/ Laneway (8m)

Figure 15 - Shared Access / Laneway (8m) cross-section

5.5. Access Restrictions

Access restrictions are applied to Glismann Road to ensure safety, maintain traffic flow, and protect sightlines, particularly on sections with steep grades or crests. The restrictions are indicated on Figure 16 and Figure 17 below. The extent of these restriction areas will be confirmed through completion of the FLP to the satisfaction of the Responsible Authority.

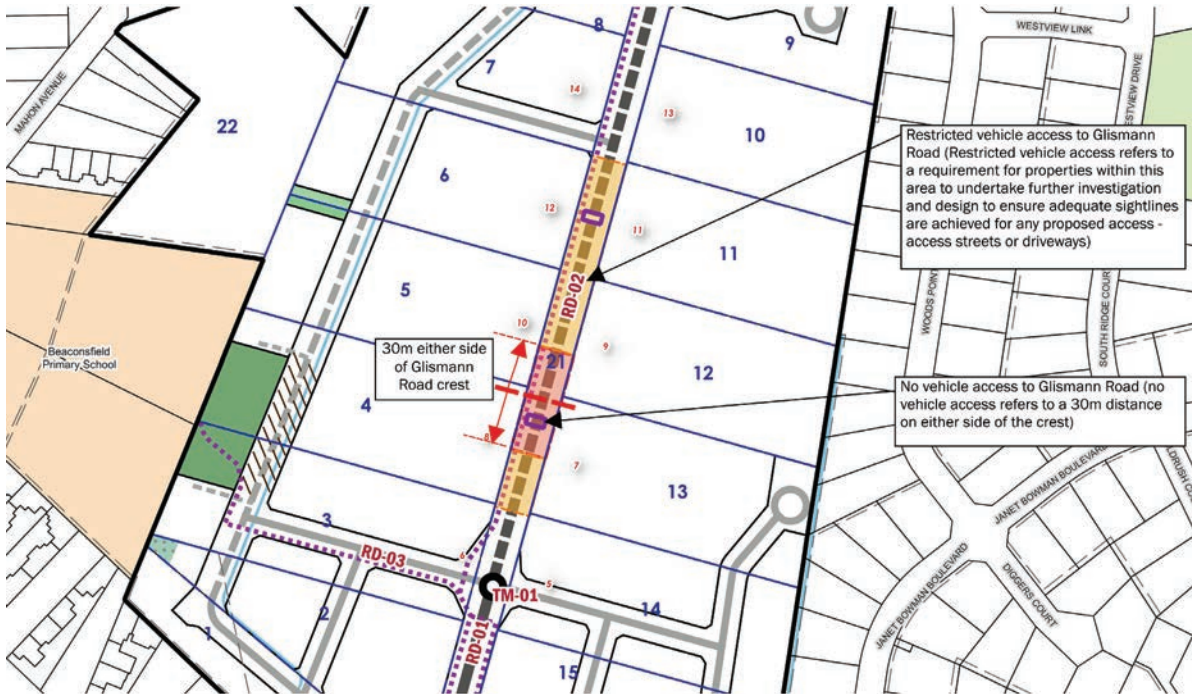


Figure 16 - Access restrictions along the Glismann Road crest

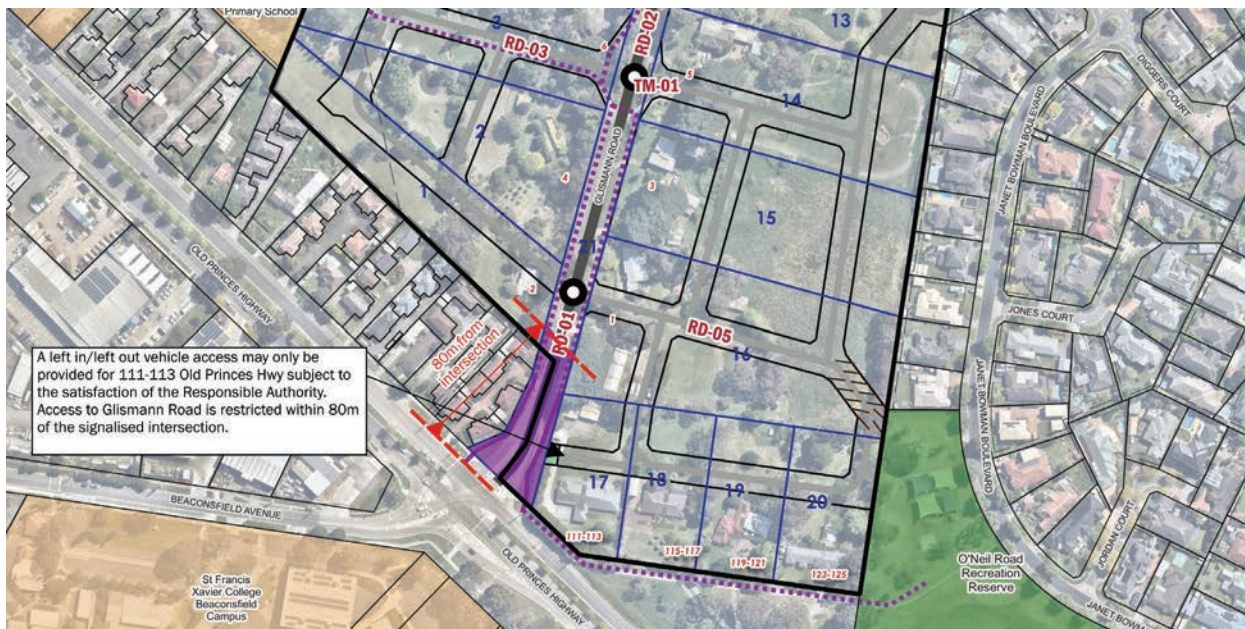


Figure 17 - Access restriction in proximity to the Old Princes Highway intersection

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Table 18 identifies areas of access restrictions, consistent with DPO19. All subdivision and development proposals must demonstrate compliance with these restrictions which are to be coordinated with other GRADP elements, including the road network, shared paths, footpaths, cut and fill, retaining wall design elements, and intersections, to ensure safe and efficient connectivity across the GRADP.

Use of non-structural, ornamental landscape elements within the road reserve to enforce low speed and deter unauthorised vehicle movements, and to define pedestrian and cyclist priority in these areas is encouraged. Vehicle exclusion barriers and landscape treatments are to be materially consistent across the GRADP.

Table 18 - Areas of Access Restriction

Area	Conditions
No Vehicle Access Permitted	<ul style="list-style-type: none"> Applies to parts of Property 4, 5, 12 and 13, or land within 30m of the Glismann Road (RD-02) crest. No vehicle access, including roads, driveways and parking, is permitted within this area, unless with the further approval of the Responsible Authority and following completion and approval of the FLP.
Restricted Vehicle Access	<ul style="list-style-type: none"> Applies to Property 3, 4, 5, 6, 11, 12 and 13. The Restricted Vehicle Access area will be refined following completion and approval of the FLP. Driveway access to new lots within this area is permitted subject to a sight distance assessment to the satisfaction of the Responsible Authority. No new road connections to Glismann Road are permitted within this area. Shared driveways or alternative access arrangements may be considered to minimise cut and fill, reduce vehicle conflicts, and maintain safe sight distances.
Restricted Vehicle Access (DTP)	<ul style="list-style-type: none"> Applies to Property 17 and part of Property 1 and 16. Access in this area is subject to DTP requirements. This may include restrictions on turning movements, limited entry/exit points, or other measures to ensure compliance with SSD and safe operation of the Old Princes Highway intersection.
Properties 17, 18, 19 and 20	<ul style="list-style-type: none"> Current access from the Old Princes Highway will be removed once these Properties are developed.
Property 22 (11 Mahon Avenue)	<ul style="list-style-type: none"> A reserve must be provided from Property 5 and 6 to Property 22 as specified in DPO19 to facilitate a pedestrian connection. The location, design and construction of the pedestrian connection must be to the satisfaction of the Responsible Authority and respond to site topography and facilitate accessible grades. This is not a DCP funded item.

5.6. Temporary Access Measures and Arrangements

- Ensuring residents can safely and reliably access their properties during construction is a fundamental requirement of the GRADP.
- No resident should be denied access at any stage of development.
- Temporary access measures are therefore mandatory and must be carefully planned and implemented by developers, to the satisfaction of the Responsible Authority.
- These arrangements provide reassurance to landowners that their needs will be respected, and that access will be maintained through all stages of subdivision and road construction.

Extent	<ul style="list-style-type: none">• Temporary court bowls must be provided for all roads where a future connection is not yet constructed.• Temporary access arrangements must be provided for properties affected by the staged delivery of RD-01 and RD-02.
Construction Trigger	<ul style="list-style-type: none">• Temporary access measures are to be implemented as part of any subdivision or road construction that affects existing access or creates an interim termination of a road connection.
Design Requirements	<ul style="list-style-type: none">• Temporary court bowls must be located within the subject property unless otherwise agreed by the Responsible Authority.• Alternative access arrangements for properties with access to Glismann Road that are not developing must be shown on a FLP and be developed in consultation with affected landowners, to the satisfaction of the Responsible Authority.• The levee bank must be designed and constructed to the satisfaction of DTP, Melbourne Water and Council. Safe vehicle access for affected properties must be maintained until direct access to Old Princes Highway is removed through their redevelopment.
Funding & Delivery	<ul style="list-style-type: none">• Temporary access works are to be delivered by the developer(s) responsible for the associated subdivision or road construction, to the satisfaction of the Responsible Authority.• These works are not funded through DCP05.

5.7. Shared Path Network

Extent	<ul style="list-style-type: none"> • DCP Project SP-01 (SP-01): located along the levee bank which runs along the frontage of Property 17 to 20, connecting through to O'Neil Road Recreation Reserve. • Glismann Road Path: a new shared path within the 20m Glismann Road reservation, connecting from the Old Princes Highway through to Patrick Place.
Construction Trigger	<ul style="list-style-type: none"> • SP-01: To be constructed concurrently with the levee bank. • Glismann Road Path: To be constructed in stages, consistent with the delivery of RD-01, TM-01 and RD-02.
Design Requirements	<ul style="list-style-type: none"> • All shared paths are to be 3m wide and constructed to IDM standards. • SP-01: <ul style="list-style-type: none"> • Located external to the GRADP area, within Crown land to the south of the Old Princes Highway properties. • To be constructed along the top of the levee bank, which will be formed with a flat top to accommodate the path. • Glismann Road Path is to be constructed on the western side of Glismann Road.
Funding & Delivery	<ul style="list-style-type: none"> • SP-01 is a designated DCPO5 item and will be delivered as required by Melbourne Water. • Glismann Road Path: <ul style="list-style-type: none"> • Glismann Road Path in RD-01: Design and construction is funded by DCPO5 and delivered as part of the RD-01 works by developers. • Glismann Road Path in TM-01: Design and construction is funded by DCPO5 and delivered concurrently with TM-01. • Glismann Road Path in RD-02: Design is funded by DCPO5 (via FLP). Construction is not funded by DCPO5 and will be delivered in conjunction with RD-02 to be constructed by developers.

5.8. Footpath Network

Extent	<ul style="list-style-type: none">• Footpaths are to be provided on both sides of all roads.• RD-01 and TM-01 include a footpath on their eastern side. No footpath is included in the RD-02 cross-section
Construction Trigger	<ul style="list-style-type: none">• Footpaths are to be constructed concurrently with the construction of the associated road.
Design Requirements	<ul style="list-style-type: none">• Footpaths are to be a minimum 1.5m wide.
Funding & Delivery	<ul style="list-style-type: none">• Footpaths associated with RD-01, RD-03, RD-04, RD-05 and TM-01 are included in DCPO5.• All other footpaths are to be delivered by the respective developer(s) at their cost.

5.9. Access to Public Transport

The GRADP provides safe and convenient pedestrian and bicycle access to existing public transport services along Old Princes Highway and Timberside Drive. Access will be facilitated via the shared path network along Glismann Road (RD-01 and RD-02) and the footpath network on all internal streets. These connections ensure residents can reach bus stops and other public transport facilities safely and efficiently. No new public transport infrastructure is proposed as part of the GRADP with access improvements delivered as part of the shared path and footpath works.

A bus stop closer to the Old Princes Highway and Glismann Road intersection would be of benefit to local residents. Council may consider advocating for this improvement with DTP.

6. Land Use Typologies

In response to the GRADP's constraints, particularly topography and vegetation, two residential typologies are identified:

- 1) Neighbourhood Residential – Conventional and medium density lots on less constrained land, integrating with surrounding development while retaining significant vegetation.
- 2) Visually Sensitive Residential – Larger lots on steeper or visually prominent land, designed to minimise visual impact and earthworks, and protect landscape and vegetation.

These typologies aim to deliver a high-quality, site responsive residential precinct that:

- Provides a diversity of lot sizes and housing opportunities to support community needs and affordability.
- Retains significant vegetation and strengthens the area's landscape character.
- Protects visually prominent hilltops and ridgelines.
- Responds sensitively to topography and natural features, minimising earthworks and visual impact.

The general application of these typologies is shown in Figure 18, with criteria, development expectations and design considerations provided in the following tables. Alternative designs may also satisfy these requirements, subject to detailed site assessments and planning approvals. Criteria are also provided for transition area between the two main typologies, for non-residential land uses and the interfaces to the Old Princes Highway and the local open space.

It is important to note that the locations of the Neighbourhood Residential, Visually Sensitive Residential, and Transition Areas shown in this development plan are indicative only and do not represent confirmed or scalable boundaries. The proportion of a property that may fall within each typology will be determined on a site-by-site basis, informed by detailed topographical and contextual analysis. The transition areas are included to illustrate that there is no fixed line separating typologies, allowing for flexibility in applying design responses that reflect site-specific conditions.

Figure 18 conceptually shows how future housing lots might be arranged within the GRADP, guiding planners, developers, and the community in visualising the potential application of the residential typologies and how development can respond to natural features such as slope and vegetation. While not a final design, the layout ensures planning decisions support a well-connected and liveable community.

Indicative lot layouts are provided for Neighbourhood Residential areas (Table 19), while Visually Sensitive Residential areas (Table 20) require a site responsive approach.

Transition areas between typologies (Table 21) are identified to manage changes in slope, vegetation and visual prominence. Some properties may contain both typologies, reflecting the application of the parameters established. Planning permit applications must demonstrate a site responsive approach that aligns with these typology requirements and the broader GRADP objectives.

An indicative lot layout has not been prepared for Visually Sensitive Residential areas due to their significant physical and environmental constraints, including steep topography, extensive vegetation, and visual sensitivity. Providing an indicative layout at this stage could be premature or misleading, as it may not reflect the development potential or constraints of individual lots. Future planning permit applications will need to demonstrate site-responsive designs that address these constraints while complying with DPO19.

This indicative layout forms the basis for more detailed subdivision design responses outlined in the following sub-sections, ensuring that future permit applications are consistent with the vision, typologies, and objectives of the GRADP and DPO19.

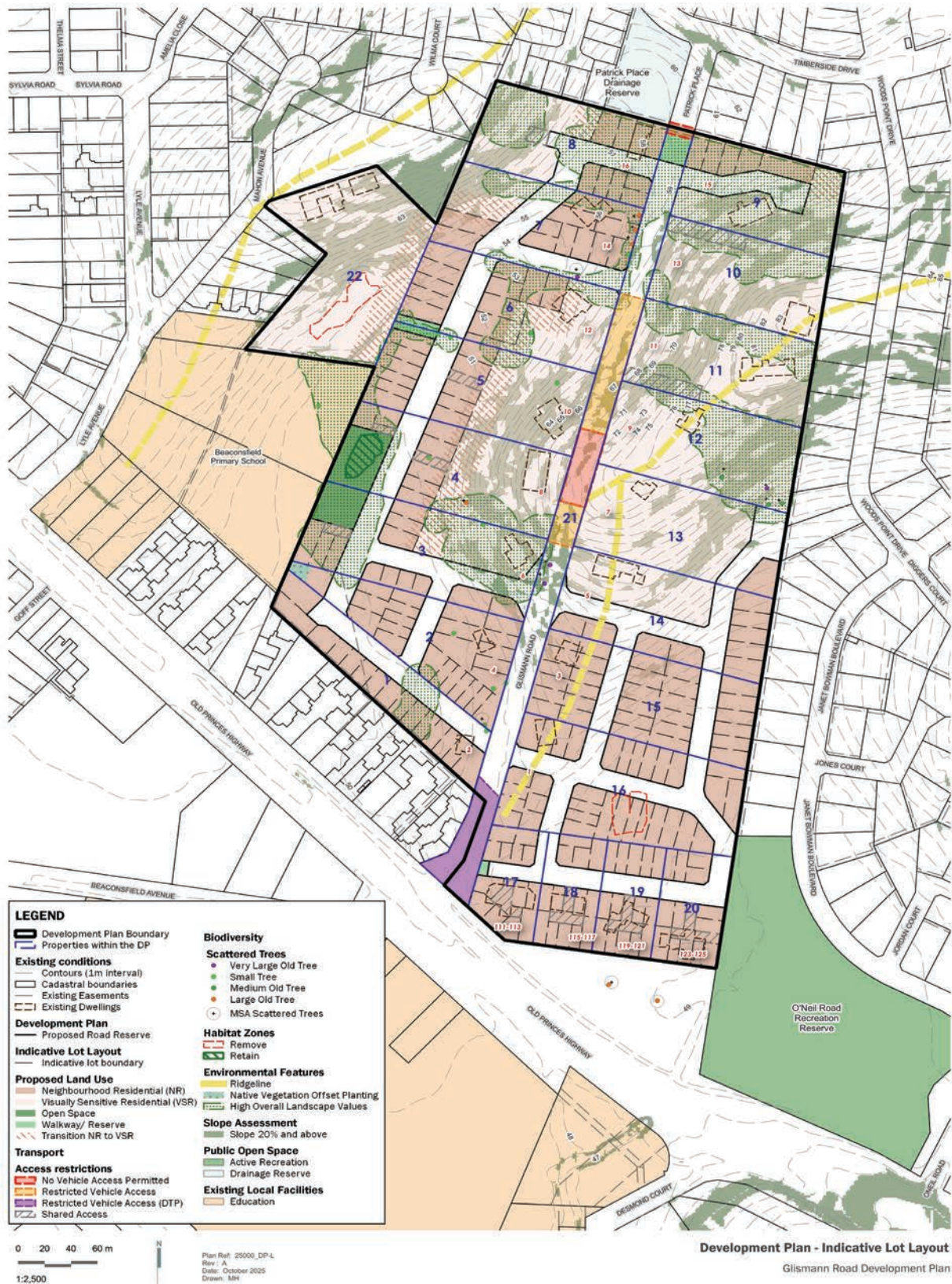


Figure 18 - Indicative Lot Layout Plan

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Table 19 - Neighbourhood Residential

<p>Applies to land that:</p>	<ul style="list-style-type: none"> • Generally has a pre-development slope of 10% or less (some limited areas of slope above 10% are included in this area). • Majority of site not located in an area with High Overall Landscape Values. • Could be developed for medium density and conventional density residential lots with minimal earthworks.
<p>Development will:</p>	<ul style="list-style-type: none"> • Provide a mix of medium density housing (townhouses and units) and conventional single dwelling lots that integrate with surrounding development and support a diverse, liveable community. • Lot sizes typically range from 250sqm – 700sqm depending on site conditions. • Retain significant trees where feasible and integrate with new canopy planting.
<p>Design considerations include:</p>	<ul style="list-style-type: none"> • Where proposals interface with existing development, ensure new development is responsive. • Ensure proposals consider retention of existing vegetation where practicable in their design. • Manage interfaces with adjacent Visually Sensitive Residential areas.

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Table 20 – Visually Sensitive Residential

<p>Applies to land that:</p>	<ul style="list-style-type: none"> • Has significant areas with a pre-development slope of greater than 10%, and areas of slope greater than 20%. • Is located in a visually sensitive area, along the ridge and sides of the ridgelines and is therefore visually prominent. • Contains High Overall Landscape Values and/or existing vegetation that should be retained and integrated into development. • Requires development approaches that minimise earthworks and visual impact.
<p>Development will:</p>	<ul style="list-style-type: none"> • Accommodate larger lots, typically 1000 sqm or greater, designed to respond to slope and vegetation. Lot sizes are to be guided by slope and building envelope location (Section 7.2.1). • Provide building envelopes to minimise earthworks and maximise retention of trees and landscape features. • Be guided by the Slope Management Guidelines (Section 8). • Not be permitted on areas with pre-development slope 20% and greater. • Need to comply with the provisions specified in DPO19, where in these areas: <ul style="list-style-type: none"> • Only one single dwelling per lot is permitted, and • Lots may not be further subdivided.
<p>Design considerations include:</p>	<ul style="list-style-type: none"> • Position building envelopes to reduce cut and fill, provide adequate setbacks from property boundaries to minimise grade differences between adjacent properties, and allow space for retention of vegetation and / or landscaping between properties. • Where existing buildings or site excavations are present, retain these features or locate new building envelopes to utilise existing cleared areas, reducing the need for additional earthworks. • Consider shared accessways to lots or dwellings to minimise driveway excavation and ensure safe sight distances along Glismann Road (where restricted access may be considered).

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Table 21 - Transition Areas

<p>Applies to land that:</p>	<ul style="list-style-type: none"> • Is identified within a Visually Sensitive Residential area with a direct interface with the Neighbourhood Residential area. • Typically has a moderate slope, and is where the flatter land transitions to land with grades in excess of 10-15%. • Allows for subdivision and building envelope design to appropriately respond to the transition between densities.
<p>Development will:</p>	<ul style="list-style-type: none"> • Provide an appropriate transition between the Neighbourhood Residential and Visually Sensitive Residential areas. • Provide building envelopes to minimise earthworks and maximise retention of trees and landscape features. • Be guided by the Slope Management Guidelines (Section 8).
<p>Design considerations include:</p>	<ul style="list-style-type: none"> • Position building envelopes to reduce cut and fill, provide adequate setbacks from property boundaries to minimise grade differences between adjacent properties, and allow space for retention of vegetation and/or landscaping between properties. • Consider shared accessways to lots or dwellings to minimise driveway excavation and ensure SSD along Glismann Road (where restricted access may be considered). • Where existing buildings or site excavations are present, retain these features or locate new building envelopes to utilise existing cleared areas, reducing the need for additional earthworks. • Manage interfaces of adjacent Neighbourhood Residential areas with Visually Sensitive Residential areas.

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Table 22 - Other Non-Residential Land Uses

Applies to land that:	<ul style="list-style-type: none"> • Has a pre-development slope generally of 10% or less. • Is not located in an area with High Overall Landscape Values. • Has direct access to Glismann Road.
Development will:	<ul style="list-style-type: none"> • Be in accordance with the provisions of the NRZ2. • Retain significant trees where feasible and integrate with new canopy planting.
Design considerations include:	<ul style="list-style-type: none"> • The decision guidelines of the NRZ2 and the ES01.

Table 23 - Old Princes Highway Interface

Applies to land that:	<ul style="list-style-type: none"> • Currently has direct interface or access to the Old Princes Highway – Property 17, 18, 19 and 20.
Development will:	<ul style="list-style-type: none"> • Remove direct access to the Old Princes Highway once development has been approved and constructed in accordance with DTP and the Responsible Authority requirements. No access to the Old Princes Highway will be permitted post development. • Provide access as follows: <ul style="list-style-type: none"> • For Property 18, 19 and 20, will gain access via Property 16. • For Property 17, left in / out only from Glismann Road subject to the approval of DTP and the Responsible Authority.

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Design considerations include:

(Refer to Figure 19, Figure 20, Figure 21)

- Medium density development, such as townhouses or units, is consistent with existing development along the Old Princes Highway and encouraged in this area (Refer Figure 19 and Figure 20 for guidance).
- Lots and buildings should provide frontage to:
 - The Old Princes Highway reserve, levee bank and shared path.
 - The O’Neil Road Recreation Reserve.
 - Any internal street created as part of the subdivision.

(Refer to Figure 20 for an example)

- Building entries (i.e. dwelling entries) should address both the Old Princes Highway reserve and O’Neil Road Recreation Reserve.
- Fencing (if provided) along the Old Princes Highway and O’Neil Road Recreation Reserve should be low (maximum 1.2m high), to clearly delineate boundaries without obscuring views.
- Property 20 must provide road access along its interface with O’Neil Road Recreation Reserve.

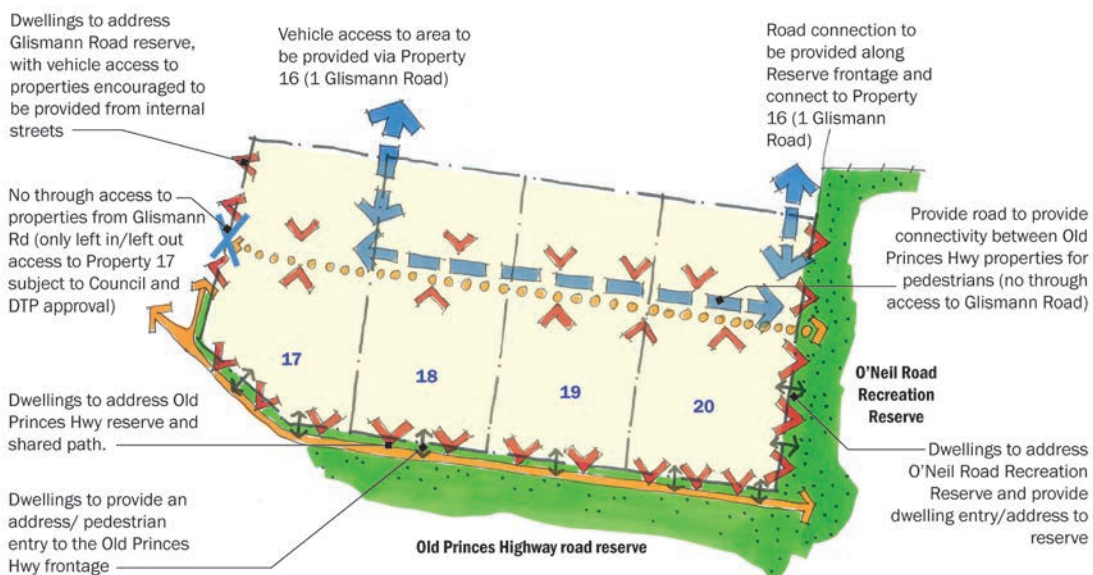


Figure 19 - Design Principles for development and subdivision of Property 17, 18, 19 and 20

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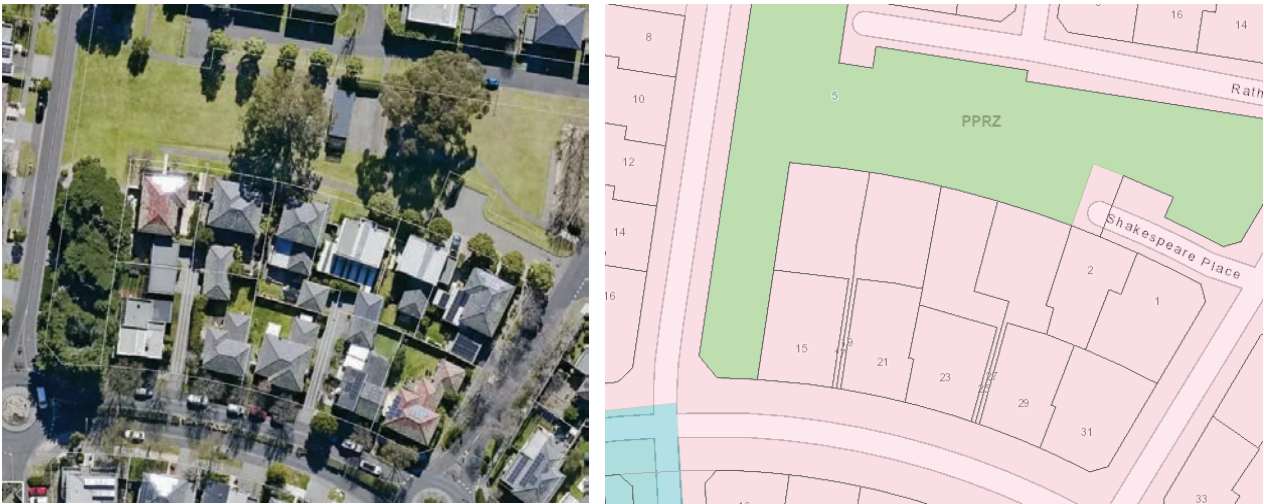


Figure 20 - Shakespeare Place, Mulgrave - 4 pack style of subdivision development, potential outcome for Old Princes Hwy properties

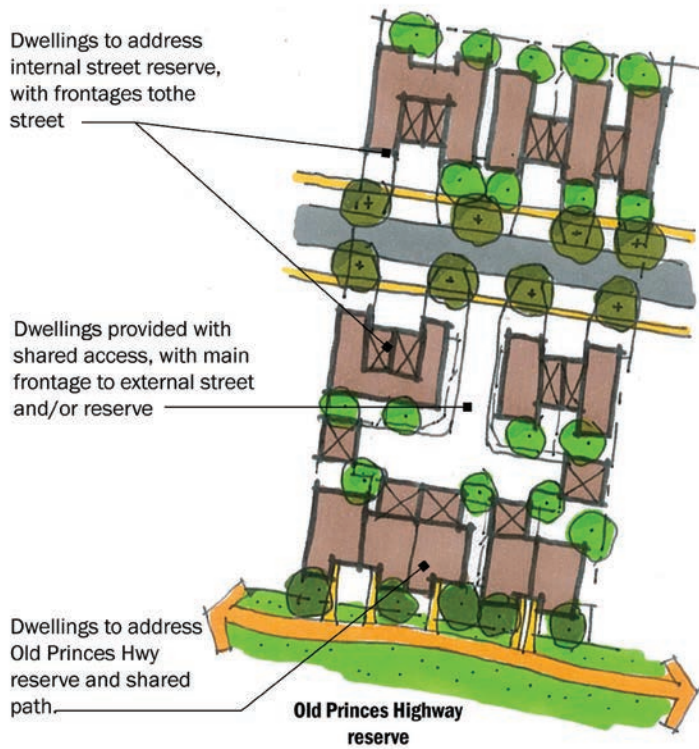


Figure 21 - Potential design outcome responding to Table 23 design considerations

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Table 24 - Local Open Space Interface

<p>Applies to land that:</p>	<ul style="list-style-type: none"> • Abuts Beaconsfield Primary School.
<p>Development will:</p>	<ul style="list-style-type: none"> • Be in accordance the desired landscape outcomes outlined at Section 10.2.
<p>Design considerations include: (Refer Figure 22)</p>	<ul style="list-style-type: none"> • Lots and future dwellings, including entries, are to front the local park, providing an active interface and encouraging passive surveillance of the open space. • Fencing, both front and side, directly on the park interface will have a maximum height of 1.2m. • No walls are to be located on the park interface. This will be achieved through building envelopes or design guidelines registered on title of new lots.

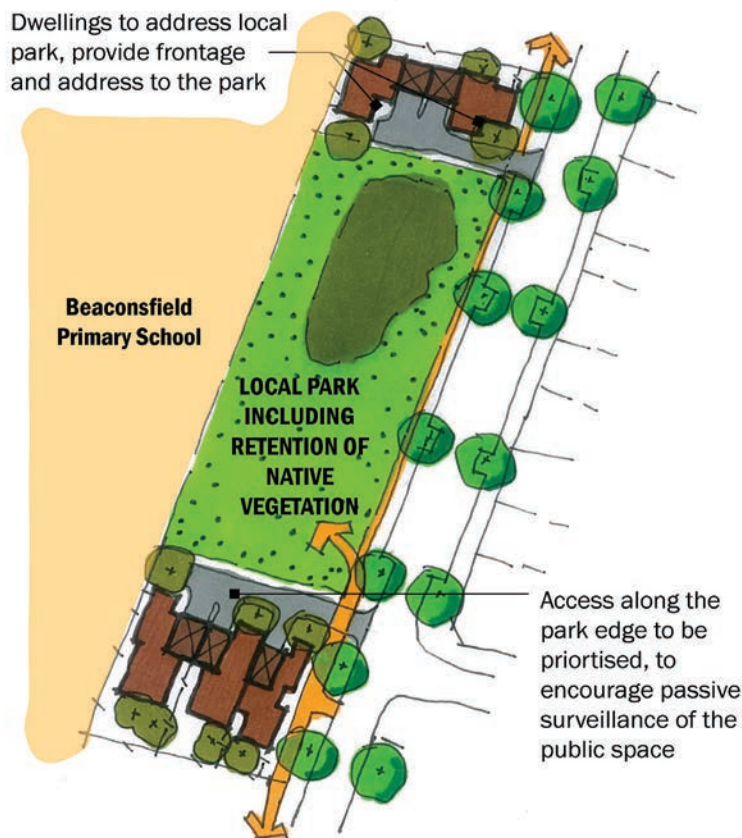


Figure 22 - Design considerations for the local park interface

7. Building Design Guidelines and Fencing Controls

Design guidelines are required by DPO19 to guide subdivision and development within the GRADP. They are intended to:

- Protect and maintain the visual prominence of vegetated hilltops and hillsides.
- Minimise disturbance to topography and existing vegetation.
- Ensure buildings, fencing and landscaping are sited and designed to avoid visually dominating the landscape.
- Deliver high quality urban design outcomes through an integrated and coordinated approach.
- Provide consistency in design outcomes and approach across the GRADP while allowing for individual site responses.
- Achieve the objectives of the NRZ2, ESO1 and DPO19 while maintaining flexibility for detailed design at the planning permit stage.

These guidelines apply to all land with the GRADP and provide a framework for the individual site Building Design Guidelines and Fencing Controls required by DPO19. Site Building Design Guidelines and Fencing Controls will be implemented together with building envelope restrictions via a condition on the planning permit that requires either a notice of restriction or an agreement with the Responsible Authority under section 173 of the *Planning and Environment Act 1987* on the certified plan of subdivision, and recorded on the Certificate of Title of the land.

Proposals must:

- Address the Overall Design Principles at Section 7.1.
- Provide building envelope restrictions and siting generally in accordance with Sections 7.2.1 and 0.
- Consider the application of land use typologies at Section 6 and topography information at Section 2.2.

7.1. Overall Design Principles

Development within the GRADP should:

- Minimise visual impact of buildings through appropriate building siting, height, scale, materials, colours, and form.
- Ensure dwellings are recessive to the landscape character and do not dominate the streetscape.
- Support passive surveillance with clearly visible front entries and window placement.
- Reinforce streetscape character through landscaping.
- Respond appropriately to topography in the design of dwellings, fencing and retaining walls.
- Minimise fencing visible from the public realm to support both streetscape amenity and passive surveillance.

The following sections provide design guidance for individual site Building Design Guidelines and Fencing Controls to meet the above principles. Alternative designs may also satisfy these principles, subject to detailed site assessments and planning approvals.

7.2. Building Design Guidelines

Building Design Guidelines are required for all subdivision and development and will be implemented via a restriction on the Plan of Subdivision or similar mechanism to the satisfaction of the Responsible Authority. The guidelines should include the following elements, as relevant to the site. Refer to Section 6 for further guidance for area typologies.

7.2.1. Building Envelopes

Building envelopes are required for all land with a predevelopment slope greater than 10%. This primarily applies to the Visually Sensitive Residential area, but may include parts of the Neighbourhood Residential area and transition area.

Building envelopes must respond to landscape character, existing native vegetation and topographical features of the GRADP. They will be implemented via a Section 173 Agreement or other mechanism to the satisfaction of the Responsible Authority.

Requirements (in addition to Clause 54 and Building Regulations):

- Must accommodate a dwelling and garage/shed. No buildings are permitted outside the building envelope.
- Protect existing trees identified for retention.
- No new buildings on land with slopes 20% or greater.
- Minimise additional earthworks by siting built form to work with the natural grade of the land.
- Avoid built form on ridgelines where practical, maintaining a minimum 3m distance below the ridgeline to allow a visible treed canopy above any buildings.
- Provide a minimum 1m setback from boundaries where retaining walls are required or integrate retaining walls with the overall building design to ensure suitable separation between building and retaining wall footings.
- Avoid built-to-boundary development on slopes greater than 10%, maintaining setbacks greater than 1m from side boundaries to retain open character and provide genuine landscape opportunities.

Figure 23 demonstrates how these principles may be applied.

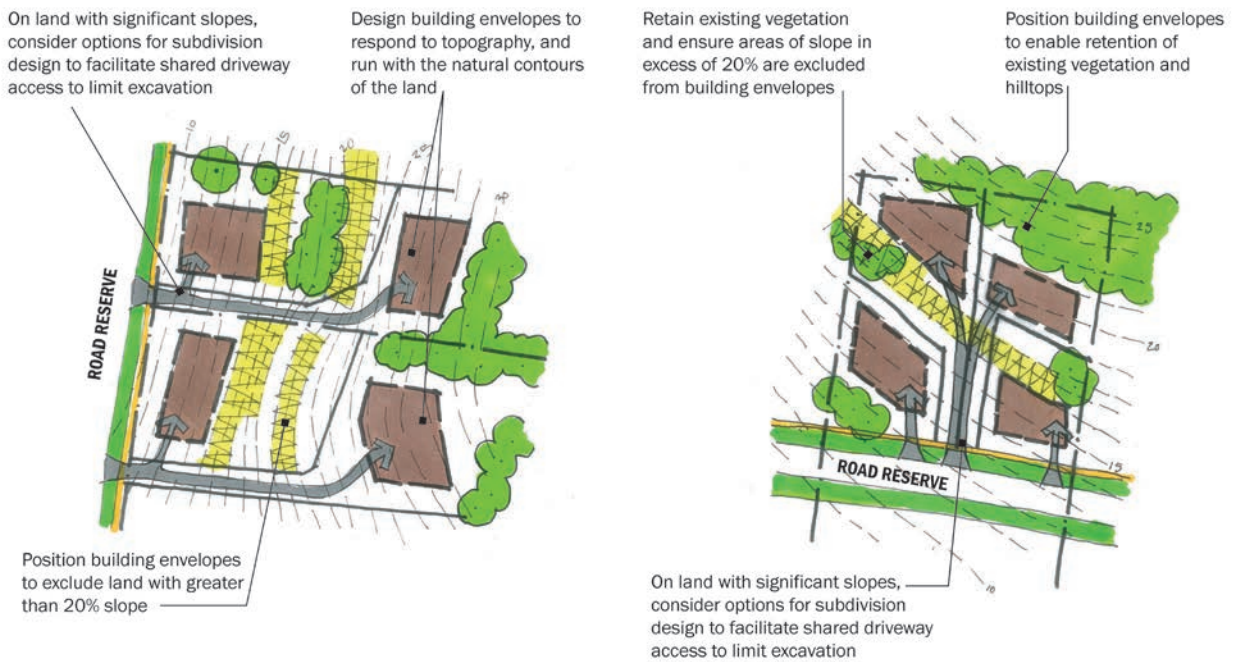


Figure 23 - Principles for siting of building envelopes

7.2.2. Building on Slopes 20% or greater

For lots created on land with pre-development grades 20% or greater, DPO19 requires that planning permits must include the following restrictions, implemented via a Section 173 Agreement or other mechanism to the satisfaction of the Responsible Authority:

- No development of land within areas of slope 20% and over.
- Only one dwelling per lot and no further subdivision for lots within areas of slope 20% and over.

7.3. Building Height

The NRZ2 includes a maximum building height of 9m, and a limit of 2 storeys at any point. Within the GRADP:

- Buildings should integrate with the landscape, and avoid dominating higher elevations (e.g. not sited on the ridgeline, height should remain below the tree canopy).
- Split level dwellings are encouraged on Very Steep and Extremely Steep land to respond to the topography and reduce overall building height.
- Proper siting of building envelopes ensures buildings sit below the tree line and blend into the landscape.

7.4. Dwelling and building design

Dwellings and buildings should:

- Integrate with surrounding landscape and topography.
- Use natural materials and muted tones that blend with the surroundings.
- Include a front entry visible from the street to support passive surveillance.
- Incorporate windows from habitable rooms along the dwelling frontage.
- Allow for retention of existing vegetation or planting of new canopy trees.
- Incorporate passive design elements supporting sustainable and energy efficient design principles.

7.5. Garages

Garages should:

- Be setback a minimum of 5.5m from the front boundary to allow for a tandem vehicle parking space in the driveway.
- Be located behind the primary building frontage of the dwelling in the Neighbourhood Residential Areas. In the Visually Sensitive Residential areas, alternative positioning of garages may be considered where this is responsive to the slope and site design.
- On lots with pre-development slopes greater than 10%, garage and driveway construction must minimise cut and fill and avoid steep gradients. Designs must be to the satisfaction of the Responsible Authority and demonstrate that it responds appropriately to site conditions and maintains safe and accessible access. A maximum driveway grade less than 20% (1 in 5) should be achieved.

7.6. Fencing Controls

Fencing should reflect the development type and respond to the surrounding landscape character.

Side and rear fencing are required to be constructed as part of subdivision works for all lots and land within the development.

7.6.1. Front fencing

- Front boundary fencing (forward of the dwelling's building line) is discouraged to maintain open, landscaped streetscapes and support passive surveillance. Refer to Figure 24.
- If front fencing is to be provided it should be low (e.g. maximum 1.2m high) and permeable.



Figure 24 - Example of a corner lot, no front fencing forward of the dwelling

7.6.2. Side and Rear fencing

- In Neighbourhood Residential areas, timber paling fencing is encouraged.
- In Visually Sensitive Residential areas, site responsive fencing (e.g. post and wire, hedging, brush fencing) is preferred.
- Side fencing should not extend past the front of the dwelling, to maintain an open streetscape.

7.6.3. Fencing of boundaries with public reserves/ land

Where lots interface with public land such as reserves and road reserves:

- Enable passive surveillance of the space by considering open style fencing elements and minimising fence height.
- Encourage fencing styles such as palisade fencing, post and wire and lower height picket fencing appropriate to the specific site context.

7.7. Retaining Walls

Retaining walls are required throughout the GRADP and are to be constructed as part of subdivision works for all lots and land within the development. They must be completed prior to issue of the relevant Statement of Compliance.

Further guidance is provided in the Slope Management Guidelines in Section 8. In relation to their materials and design:

- Where retaining walls interface with a street or public land, including alongside boundaries within the front setback of a lot, they should:
 - Use local natural rock or stone.
 - Maintain consistent design throughout the GRADP.
 - Potential examples of preferred materials are shown in Figure 25.



Figure 25 - Rock retaining wall along property street frontage, including along the street corner side boundary

- Where retaining walls are located at the rear of lots or behind the building line of a dwelling, they may be constructed from:
 - The same local natural rock or stone as above, or
 - Concrete sleepers (refer to Figure 26 for an example).



Figure 26 - Example of rock retaining wall along the property frontage and initial front setback, with concrete sleeper retaining wall behind the building line

7.8. Tree Canopy Requirements

Landscaping within future lots should:

- Encourage indigenous and native plants.
- Provide a minimum of one new canopy tree in both the front and rear of each lot, or retain existing canopy trees.
- Comply with Table 25 (Table 1 to Clause 52.37 Canopy Tree) for minimum canopy tree requirements:

Table 25 - Table 1 from Clause 52.37 Cardinia Planning Scheme

Minimum canopy tree requirement	
Site area	Canopy tree requirement
100 square metres or less	1 tree
101 sqm to 200 sqm	2 trees
201 sqm to 500 sqm	3 trees
501 sqm to 700 sqm	4 trees
701 sqm to 1000 sqm	6 trees
Above 1001 sqm	A total canopy cover within the site that is equal to at least 20 percent of the site area.

Streetscape landscaping is addressed in Section 10.4.

8. Slope Management Guidelines

The GRADP features varied topography, including significant areas of slope that influence both the form and feasibility of development. Careful design responses are required to minimise earthworks, manage visual impact, and maintain slope stability. The Indicative Lot Layout Plan (Figure 18, Section 6) provides a conceptual response to slope, identifying areas requiring specific slope management approaches, particularly within the Visually Sensitive Residential area. Figure 27 demonstrates how the GRADP has responded to the pre-development slope.

DPO19 requires Slope Management Guidelines for the subdivision and/or development of land within a pre-development slope greater than 10%.

The Slope Management Guidelines must:

- Include a statement of how the guidelines respond to the visual sensitivity of the area, topographical features and retention of areas with significant vegetation and other identified characteristics identified in the GRADP.
- Include Slope Management Design Principles for the matters identified in DPO19 as relevant to the specific site.
- Ensure development responds to the natural topography of the land.
- Minimise overall earthworks and disturbance to the landscape.
- Support siting and design of buildings, driveways, and infrastructure that integrates with the landform and retains landscape character.
- Deliver high quality urban design outcomes through an integrated and coordinated approach.
- Provide consistency in slope management outcomes across the GRADP while allowing for individual site responses.
- Achieve the objectives of the NRZ2, ESO2 and DPO19 while maintaining flexibility for detailed design at the planning permit stage.

Areas of slope greater than 20% must also include a Slope Management Plan. In these areas, DPO19 requires a condition that either an envelope/notice of restriction or an agreement with the Responsible Authority under section 173 of the *Planning and Environment Act 1987* on the certified plan of subdivision, and recorded on the Certificate of Title of the land be implemented for the following:

- Prevent development on areas with slope 20% and greater.
- Allow only one single dwelling on each lot with slope 20% and greater.
- Prohibit further subdivision of land with slope 20% and greater.

Where Slope Management Guidelines are required, proposals must:

- Address the Overall Design Principles provided in Section 0.
- Demonstrate an appropriate slope management approach and design response generally in accordance with the guidance below.
- Consider the application of land use typologies at Section 6 and topography information at Section 2.2.

8.1. Overall Design Principles

Development in areas with pre-development slope over 10% should:

- Avoid locating roads on land categorised as Very Steep or Extremely Steep (in Table 4).
- Minimise Glismann Road grades while meeting sight distance and safety requirements.
- Contain development within building envelopes in Visually Sensitive Residential areas.
- Prevent any development on Extremely Steep land.

The following sections provide design guidance for individual site Building Design Guidelines and Fencing Controls to meet the above principles. Alternative designs may also satisfy these principles, subject to detailed site assessments and planning approvals.

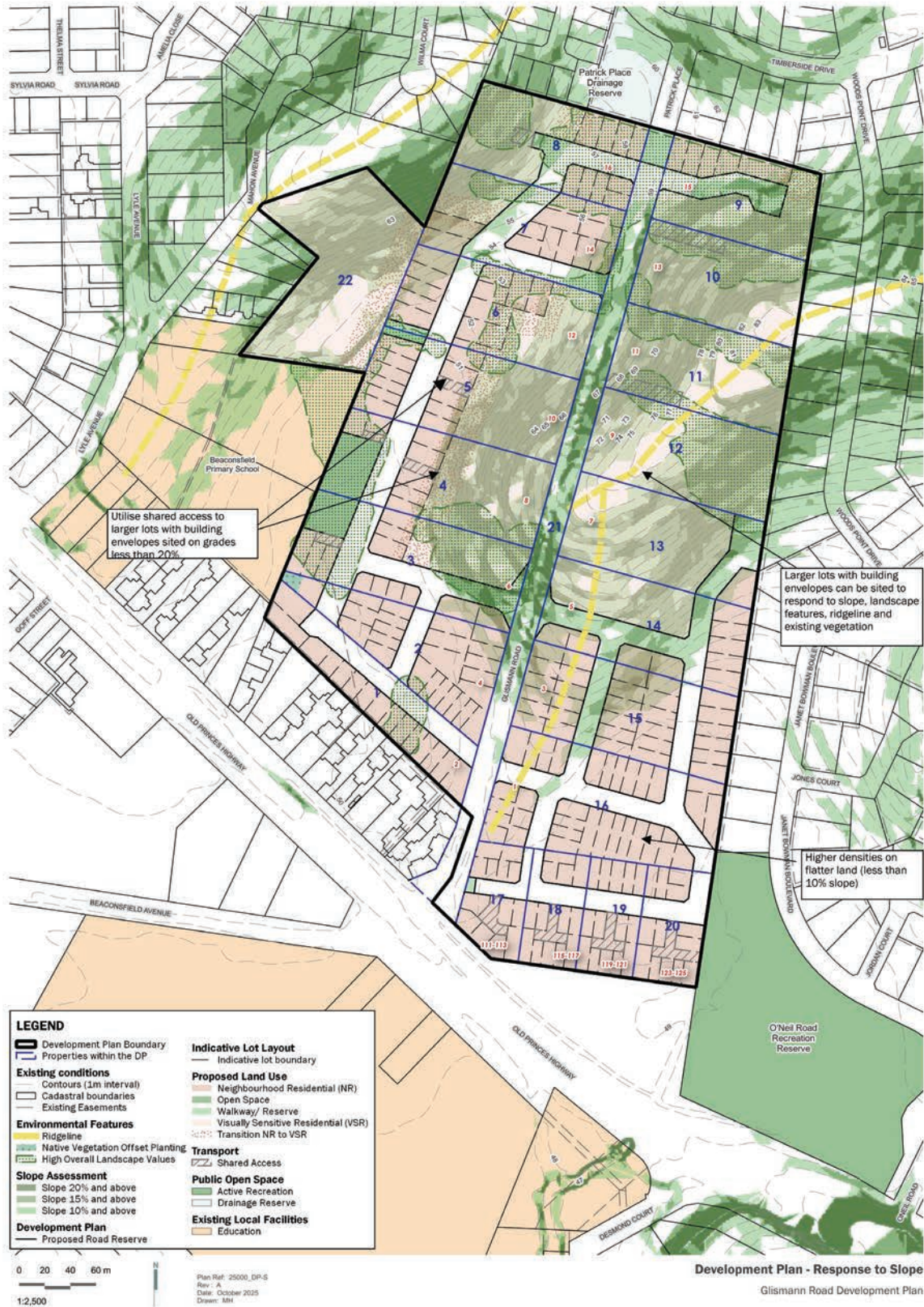


Figure 27 - Design Response to Slope

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8.2. Street layout and site access design

Streets and driveways should respond to the natural topography to minimise earthworks and integrate with the landform:

- Design streets to run along the natural contours, or run up/down the contour to balance access and slope management (refer to Figure 28)
- Consider how street grades impact lot earthworks.
- Locate driveways to minimise steep gradients, with a maximum grade of less than 20% (1 in 5), to the satisfaction of the Responsible Authority.
- For lots with side-to-side slope, place driveway crossovers on the low side of the lot to ensure the driveway can be constructed with a minimal grade
- In Visually Sensitive Residential areas, consider shared driveways to reduce earthworks and support integrated design approaches to managing slope.

Figure 28 demonstrates how these principles may be applied.

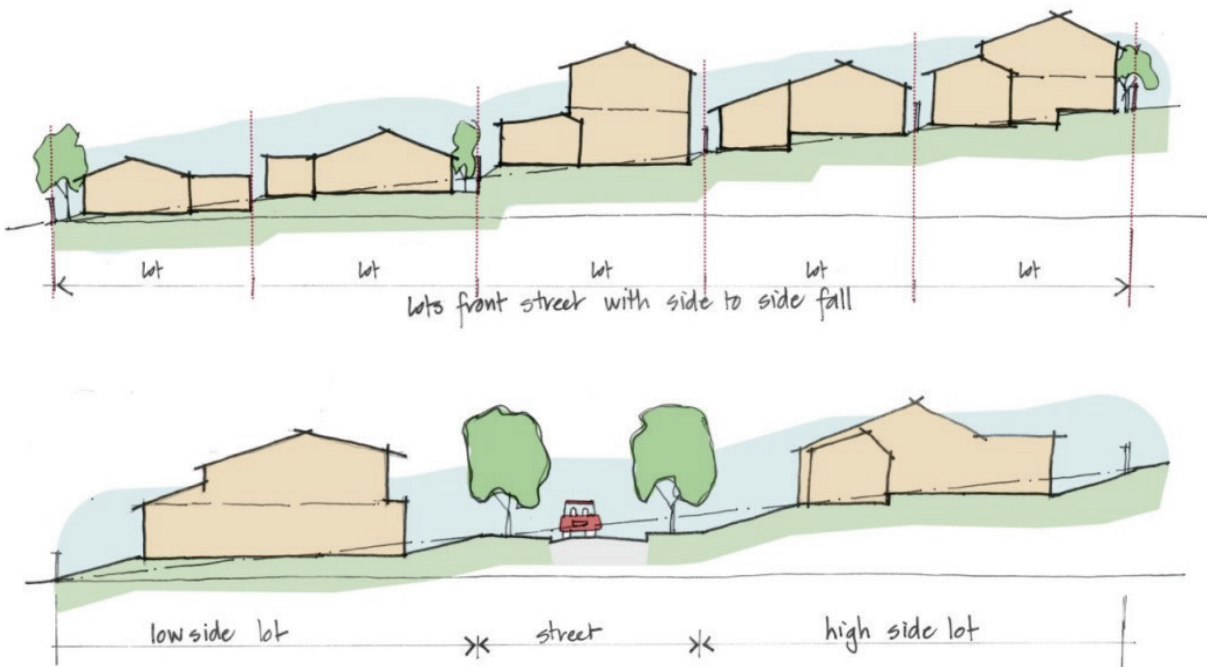


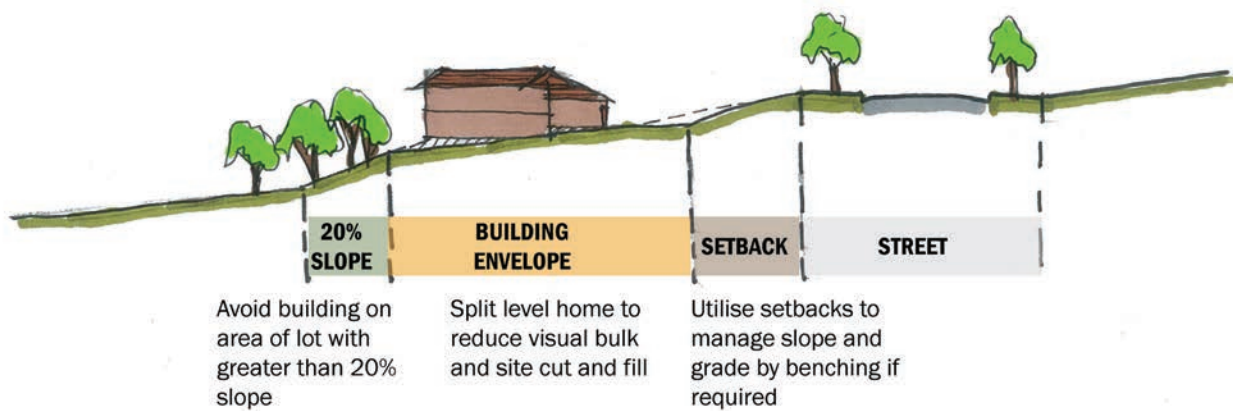
Figure 28 - Cross-section view, (1) Streets running up/down the contour and (2) Streets running along the contour

8.2.1. Lot Layout and Building Design

Lot and building design should integrate with street and earthworks design (Refer to Figure 29):

- On streets running up slopes, lot shapes should allow for building designs to manage the side-to-side fall of the lot through the minimisation of retaining wall height and benching between lots. Options may include:
 - Wider frontages to provide for building separation and landscaping between lots.
 - Narrower frontages to minimise retaining wall and fence heights.
 - For lots with slope greater than 10%, accommodate a minimum 10m x 20m building envelope without compromising secluded private open space areas, setbacks to retaining walls, landscaping opportunities or access around the dwelling.
 - Encourage split-level dwellings on Very Steep lots to minimise site works.

LOTS AND DWELLINGS ON THE LOW SIDE OF THE ROAD



LOTS AND DWELLINGS ON THE HIGH SIDE OF THE ROAD

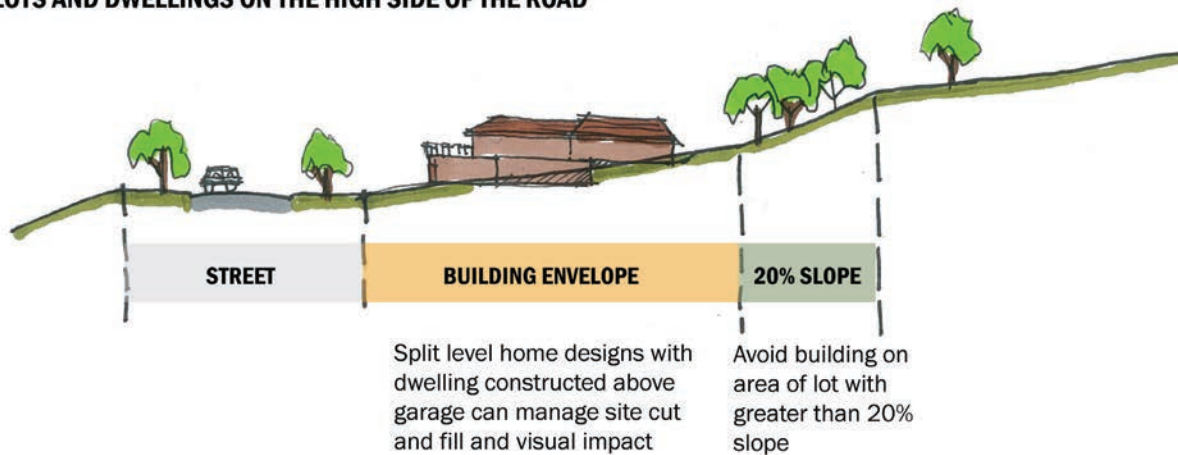


Figure 29 - Dwelling design in relation to the street

8.3. Earthworks

Earthworks, including cut and fill, benching and retaining walls are required to support development and must be designed in accordance with the following principles:

- Where streets run along natural contours, earthworks should:
 - Minimise the extent to which dwellings on the lower side of the street sit significantly below the designed street level.
 - Avoid excessive filling that causes dwellings on the higher side of the street to dominate the streetscape.
- Earthworks should be planned across multiple lots within a subdivision (and where possible consider adjoining properties) to achieve consistent, integrated outcomes.
- Earthworks should not exceed 1m cut or fill above or below natural ground level unless justified by engineering or site-specific constraints, to the satisfaction of the Responsible Authority.
- All lot earthworks are to be planned and undertaken as part of subdivision construction to ensure consistent outcomes.



On lots with grades over 10% with side to side fall, driveways on the low side allow for split level design responses that limit retaining walls on boundaries



Where lots fall away from the road, ensure the dwelling maintains a street presence and the driveway is a manageable grade

Figure 30 - Appropriate design and siting responses to slope



Split level home on lot with side to side grade on pre-development slope in excess of 20% still requires significant cut and fill



Avoid subdivision of small lots on steep (in excess of 20% grade) land

Figure 31 - Examples of impact of developing on land with greater than 20% slope

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8.3.1. Lot Benching

Lot benching:

- Provides flat pads for building, reducing the need for retaining walls.
- Improves housing affordability by enabling standard builder homes to be constructed without the need for significant cut and fill, which would otherwise add to construction costs or require individually designed split-level dwellings to accommodate the site constraints.
- Should be designed and located based on the following key principles (refer to Figure 32):
 - Benching may be used to take up slope in front, rear, or side setbacks.
 - Benching is particularly useful in sloping areas of up to 10% grade, and can be integrated with retaining walls on steeper slopes.
 - Batters must not exceed a 1:4 (25%) gradient.

The Slope Management Guidelines must show the location of all batters and benched areas, including gradients and dimensions.

All lot earthworks are to be planned and undertaken as part of the subdivision construction to ensure consistent outcomes.



Subdivider has pre-benched and included retaining walls and fences on lots to ensure suitable buildable pads for dwellings



Where pre-development slope is less than 10%, consider how pre-benching lots and including retaining walls could manage site cut and fill and limit conflict between lots

Figure 32 - Benching and retaining walls undertaken by subdivider/ developer

8.3.2. Retaining walls

Retaining walls manage grade change and provide suitable buildable areas for residential lots. They should be designed and located based on the following key principles (refer to Figure 33):

- Retaining walls visible from public spaces (e.g. parks, streets, footpath) should not exceed 1m in height.
- Retaining walls on boundaries should be setback at least 1m from building envelopes/ dwelling footprints, or integrated with the overall building design, to avoid conflict between footings.
- In Visually Sensitive Residential areas, retaining walls must minimise cut and fill impacts on adjoining building envelopes by:
 - Staggering walls with at least 1m between each stagger to allow landscaping.
 - Ensuring drainage infrastructure and structural foundations are fully located within the lot.
 - Restricting the maximum overall height for a staggered wall to 2m.

- Fences located directly above or within 1 m of retaining walls should be limited to 1.8 m to reduce overshadowing.

The Slope Management Guidelines must indicate the location, height and proposed materials for all retaining walls. All lot earthworks, including fencing must be planned and undertaken as part of the subdivision construction to ensure consistent outcomes. All retaining walls on lot boundaries are to be planned and undertaken as part of the subdivision construction to ensure consistent outcomes.



Avoid built to boundary walls along retaining walls and steep land



Ensure side boundary retaining is coordinated along lot boundaries to avoid excessive cut and high retaining walls

Figure 33 - Retaining wall outcomes to be avoided on land with steep slopes

8.3.3. Buildable Areas/ Building Envelopes

- Buildable areas / building envelopes lots should be shown on the SMP, indicating the area and dimensions available for construction.
- Design should integrate proposed retaining walls and lot benching to ensure sufficient space for dwellings, landscaping, access, and setbacks.

8.4. Requirements for a Slope Management Guidelines

A Slope Management Guidelines must be submitted within any planning permit application for land with pre-development slopes greater than 10%, as prescribed by DPO19. The Slope Management Guidelines must include the following information:

- Slope Analysis:
 - A plan showing pre-development slope, categorised as less than 10%, 10-15% and 15-20% and greater than 20% (per Table 4).
 - Identification of natural topography, historic and recent earthworks or fill, and any proposed works to alter ground levels and areas of substantial slope.
- Design Response:
 - A clear explanation of how the subdivision layout responds to slope conditions and integrates slope management works with adjacent land.
 - Details on how areas of Extremely Steep slope will be managed, avoided, or mitigated through the SMP. Development is not permitted on areas of Extremely Steep slope.

- Road and Access Design:
 - Proposed road cross sections and long sections demonstrating how slopes greater than 10% are addressed in road alignment and grading.
 - Driveway crossover locations and design details to ensure safe and practical access consistent with slope management objectives.
- Earthworks and Retaining Structures:
 - Details of all proposed cut and fill works, batters, retaining walls, and associated drainage.
 - Retaining wall information including height, staggering, finished levels, construction materials, and fencing.
 - Drainage solutions for land with an existing pre-development slope of greater than 10%.
- Landscape and Urban Design Response:
 - Plan and/or statements showing how landscaping and built form outcomes will integrate with slope management measures and soften the visual impact of retaining walls and batters.
- Geotechnical Assessment:
 - A geotechnical report and relevant engineering designs, prepared by a suitably qualified engineer, confirming slope stability and assessing risks associated with proposed earthworks and drainage (unless otherwise approved by the Responsible Authority).
- Building Envelopes:
 - Details demonstrating how proposed building envelopes (or an alternative design response) respond to the slope management methods utilised, and have informed the subdivision layout.

All lot earthworks and side and rear fencing are to be planned and undertaken as part of the subdivision construction to ensure consistent outcomes.

9. Vegetation

The GRADP supports a mix of remnant native vegetation, planted trees, and modified grassland reflecting its rural and residential history. These features contribute to both its ecological and visual character and require careful management through subdivision and development, including the:

- Sensitive integration of existing vegetation within subdivision and development design.
- Appropriate offsets where vegetation removal is unavoidable.
- Establishment of new street and reserve plantings using locally indigenous species (refer to Section 0).

Figure 34 establishes the Vegetation framework for the GRADP by illustrating:

- Habitat Zone and Scattered Tree information from the Ecological Heritage Partners (2010) Biodiversity Assessment.
- High Landscape Value areas identified in the Hansen Partnership (2014) Landscape Values Assessment.
- Relevant Melbourne Strategic Assessment (MSA) information.

GRADP vegetation outcomes must also be consistent with the relevant provisions of the Cardinia Planning Scheme including:

- NRZ2 – schedule objectives.
- DPO19 – schedule objectives and application requirements.
- ES01 – statement of environmental significance, environmental objectives, and permit requirements.
- Clause 52.17 – permit requirements relating to native vegetation removal.
- Clause 52.37 – permit requirements relating to canopy trees.

As required under DPO19, any planning permit application must include:

- A current flora and fauna assessment preparing by a suitably qualified ecologist, addressing and updating the findings of the Ecology and Heritage Partners (EHP) (2010) Biodiversity Assessment.
- A Native Vegetation Information Management (NVIM) report confirming the extent of native vegetation to be removed, retained, or offset.

The design response accompanying a planning permit application must:

- Be informed by current flora and fauna and arboriculture assessments.
- Demonstrate how the subdivision and built form will retain and protect significant vegetation and manage biodiversity values.
- Integrate vegetation retention, landscape design, and drainage outcomes to achieve a coordinated and sustainable development pattern.

The following subsections outline the key biodiversity and landscape elements to be addressed through the planning and design process. These requirements should be read in conjunction with Sections 7 and 8 to ensure a consistent approach to built form and landscape integration.

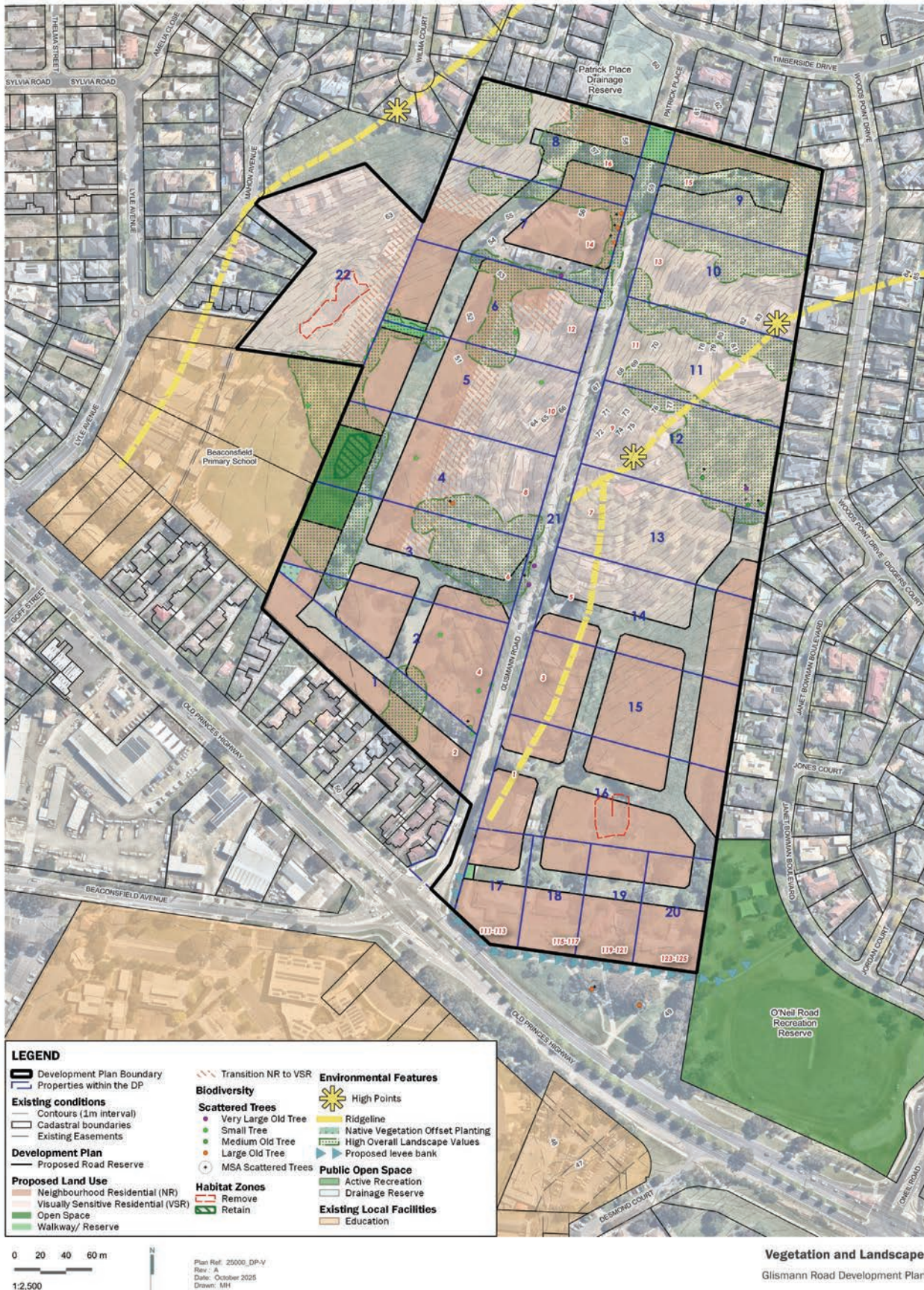


Figure 34 - Vegetation Plan

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9.1. Habitat Zones

The *EHP (2010) Biodiversity Assessment*, as summarised in the KIP, identified three habitat zones within the GRADP comprising remnant native vegetation and scattered trees across modified grassland (refer to Figure 3).

A small patch of Swampy Riparian Woodland (EVC 83, Habitat Zone 2A (HZ2a)) associated with the southern drainage line is to be retained within the local open space (LA-05a and LA-05b). Retention of this area provides an opportunity to:

- Protect remnant native vegetation.
- Maintain habitat connectivity.
- Integrate natural drainage and landscape values within an area of public open space.

Design and management considerations for LA-05a and LA-05b, including the secure conservation zone around HZ2A, staging of the delivery of LA-05a and LA-05b, buffers, and access arrangements, are provided in Section 10.2.

The other two Habitat Zones, located on Property 16 and Property 22 are not proposed for retention under the GRADP. Their removal will be subject to a planning permit and detailed biodiversity assessment at the time of application.

9.2. Scattered Trees

In addition to the identified Habitat Zones, the GRADP contains Scattered Trees that contribute to its landscape character and ecological connectivity.

The *EHP (2010) Biodiversity Assessment* identified 26 Scattered Trees within the GRADP, ranging from 'Small' to 'Very Large Old Tree'. An Arboriculture Assessment is required for each site with any planning permit application to provide current, site-specific information to inform the design response.

Retention Guidance (starting position):

- Very Large Old Trees and Large Old Trees should generally be retained within public land, including extended road reserves where feasible.
- Where such trees are retained on private lots, building envelopes must not extend into the Notional Root Zones (NRZ), and protection measures secured on title.
- Medium and Small Trees should be retained where practicable, particularly if they contribute to streetscape or ecological values.

9.3. Areas of High Landscape Values

The 2014 Hansen Partnership Landscape Assessment identified areas of high overall landscape value, shown in Figure 34. These areas are typically within Visually Sensitive Residential areas as established in Section 6.

The Landscape Assessment identifies these areas as:

High landscape value is associated with the Small Acreage: Bushland areas, which are scattered throughout the study area. This level of relatively high value was largely assigned due to the presence of groups of well established native vegetation.

Planning permit applications must demonstrate a design response that is informed by a site survey, arboriculture assessment, and flora and fauna assessment to establish appropriate design parameters.

Retention of existing vegetation in these areas is a primary consideration and must be guided by the ESO1, and Clauses 52.17 and 52.37 of the Cardinia Planning Scheme.

Building envelopes and site design must:

- Be responsive to vegetation retention, existing topography, and the need for retaining walls or other site works.
- Minimise vegetation removal through alternative layout and design solutions.
- Integrate scattered and canopy trees into subdivision and building design wherever feasible.

Design Considerations:

- Prioritise retention and rehabilitation of significant vegetation as identified by the design response, particularly in Visually Sensitive Residential areas.
- Maintain and enhance overall vegetation coverage, avoiding incremental or ad-hoc clearing.
- Use informal, appropriately scaled landscaping to screen buildings, structures and large areas of hard surfaces, consistent with the area's landscape character.
- Remove environmental weeds and replace with local native and indigenous species, particularly those that are drought- resistant and have fire retardant properties (refer CFA, 'Landscaping for Bushfire' document).
- Align both private and public landscaping with existing landscape character to reinforce visual and ecological connections.
- Strengthen vegetative linkages to natural features such as creeks, drainage lines, and open spaces.
- Ensure all trees identified as Canopy Trees, Very Large Old Trees or Large Old Trees are appropriately retained and protected.

9.4. Canopy Trees

Trees identified as Canopy Trees under Clause 52.37 are encouraged to be retained, with protection measures recommended by an arboriculture assessment and implemented through lot and building siting and construction management plans.

9.5. Notional Root Zone

Development and construction activities must avoid disturbance within the Notional Root Zone (NRZ) of retained trees to protect their long-term health and structural stability. Works within the NRZ, including excavation, filling, installation of services, or vehicle movement, should be minimised and undertaken in accordance with AS 4970–2009 *Protection of Trees on Development Sites*.

Prior to the commencement of works, temporary fencing and clear signage must be installed to delineate exclusion zones and prevent compaction or damage to tree root systems.

9.6. Melbourne Strategic Assessment (MSA) Program

The GRADP is located within the Melbourne Strategic Assessment (MSA) levy area. The MSA Program allows developers to pay a levy to 'offset' biodiversity impacts, with funds directed towards conservation reserves and conservation management programs elsewhere in Victoria.

All properties within the GRADP are required to pay the MSA levy, and related restrictions may already be registered on property titles. Individual Property levy's can be checked here -

<https://mapshare.vic.gov.au/msa/>

Payment of the MSA levy does not exempt properties from the need for a planning permit to remove, destroy or lop vegetation. All proposals must continue to address the vegetation retention, assessment, and design response requirements outlined in the GRADP.

9.7. Ongoing Management

Ongoing management of retained vegetation and public open space areas must be addressed through a Landscape Management Plan or Conservation Management Plan to the satisfaction of the Responsible Authority. These plans should outline maintenance responsibilities, weed and pest management, replanting strategies, and long-term monitoring to ensure the persistence of vegetation values over time.

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10. Landscape

The landscape of the GRADP is a defining element of its visual and landscape character. It reflects the interplay between natural landform, vegetation, open space, and underpins its distinct foothill identity within Beaconsfield.

The Landscape Management Plan (LMP), shown at Figure 35, establishes the framework for vegetation retention, revegetation, and the delivery of high-quality public open space that integrates with the GRADP's topography and urban context.

Landscape design within the GRADP must:

- Respond sensitively to its varied topography and natural drainage patterns.
- Protect and enhance significant vegetation and ecological connections.
- Reinforce the natural and rural character of the Beaconsfield hillsides, as identified in ESO1.
- Contribute to stormwater management, biodiversity outcomes, and visual cohesion across public and private interfaces.

These landscape guidelines support the objectives of DPO19 and the NRZ2, providing a framework for coordinated landscape outcomes while allowing flexibility for detailed design at the planning permit stage.

In accordance with DPO19, a LMP must accompany planning permit applications and demonstrate an integrated approach to vegetation protection, slope responsive landscaping, and the creation of a high quality public realm.

10.1. Revegetation

Existing mature vegetation and steeply sloping land are critical to maintaining and enhancing the landscape character of the GRADP. Where vegetation is identified for retention, any landscape plans must include a Revegetation Plan prepared by a suitably qualified landscape designer.

The Revegetation Plan must:

- Set out a staged approach to tree protection, weed management, erosion control and succession planting.
- Preserve areas of high visual exposure and / or ecological value through a long-term planting strategy that replaces existing vegetation over time.
- Consider forecast climate change impacts on vegetation performance and the habitat needs of local fauna when selecting a planting palette.

Planting palettes must be informed by relevant local Ecological Vegetation Class (EVC) Benchmarks and specify plant stock of local provenance for installation.

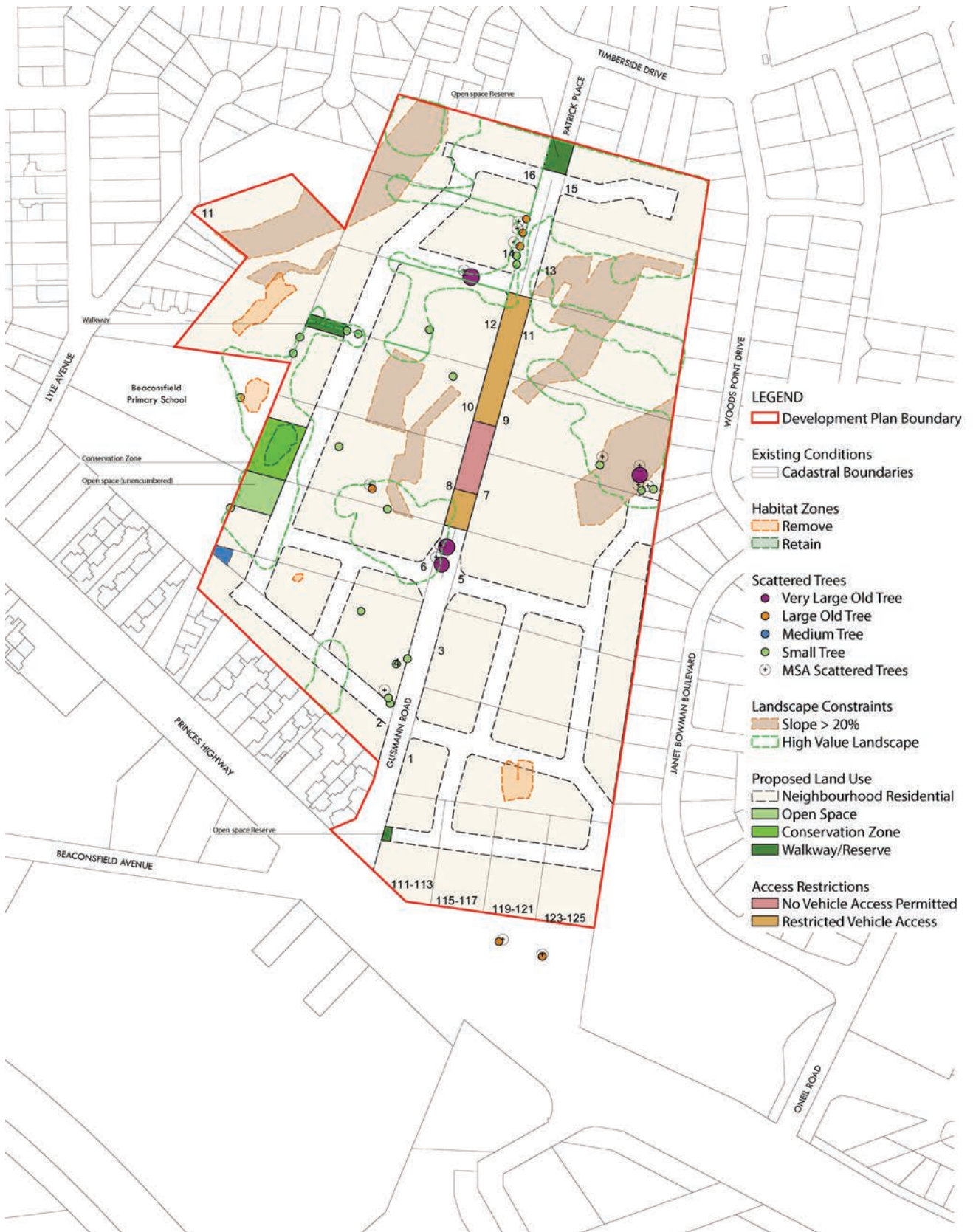


Figure 35 - Landscape Management Plan

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10.2. Open Space

Public open space within the GRADP contributes to landscape character, community amenity, and recreational opportunities. An area of public local open space (LA-05a and LA-05b) within the GRADP is established by DPO19 and DCP05, with DCP05 also providing for local open space embellishments (OSLP-01). The GRADP also directly abuts O'Neil Road Recreation Reserve, with contributions towards reserve and pavilion upgrades also provided by DCP05 (DCP Projects OSNR-01 and CI-01). Figure 36 identifies the location of open space within the GRADP.

Under DCP05, the local open space must be delivered by Property 3 (LA-05a) and Property 4 (LA-05b). For this development plan, when referring to the local open space component it will be referred to as LA-05 (rather than LA-05a and LA-05b), unless specific reference is required to Property 3 or Property 4.

As required by DPO19, the first application to subdivide land containing LA-05 must be accompanied by an indicative concept master plan for the entire local park. This plan must demonstrate how this local open space will be delivered and developed in stages, ensuring that each stage provides a functional and accessible open space outcome for each of Property 3 and Property 4.

The delivery of the local open space should not be dependent on the subdivision or development of both Property 3 and Property 4. An initial concept plan to guide future development of the DCP components of LA-05a, LA-05b and OSLP-01 is provided in Figure 37.

Design Considerations:

- Provide direct road frontage to LA-05 with on-street parking provided on both sides of the carriageway. Refer to Table 24 for further information.
- LA-05 must include a conservation zone incorporating the patch of Swampy Riparian Woodland (EVC 83, Habitat Zone 2A, refer to Section 9.1). This conservation zone must be securely fenced to the satisfaction of the Responsible Authority with no public access permitted. Fencing should be visually recessive while allowing maintenance access.
- The balance of LA-05 is to be developed as a neighbourhood park incorporating play equipment, seating and landscaping. A buffer (minimum 10m) of short-mown grass or non-flammable groundcover must separate the play area from the conservation zone fencing and be managed as defensible space.
- Where LA-05 adjoins Beaconsfield Primary School, a secure palisade style fence (Refer Figure 38 for example image) must be constructed. A lockable gate may be provided to enable controlled pedestrian access during school drop-off and pick-up times, subject to agreement with the school.
- Provide pedestrian and cycling connections linking LA-05 to surrounding streets, the proposed shared path network, and O'Neil Road Recreation Reserve.
- Use surface treatments, planting, and park furniture that reinforce the landscape character and complement the adjoining conservation area.
- Integrate drainage and water-sensitive urban design (WSUD) elements with landscape design to maintain appropriate hydrological conditions for the retained Swampy Riparian Woodland.
- Ensure staged delivery provides a functional, safe open space, including landscaping and seating.

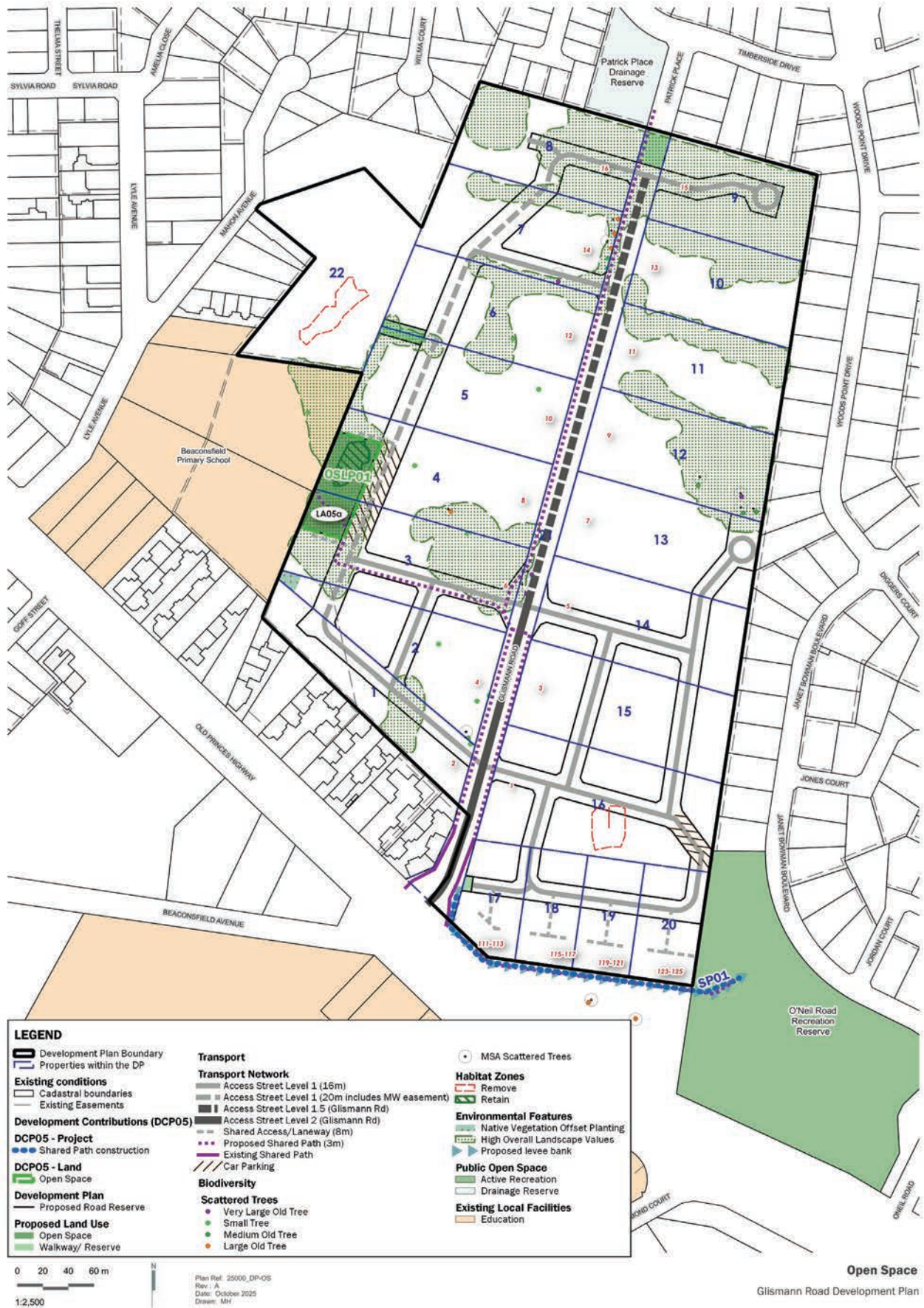


Figure 36 - Open Space Plan

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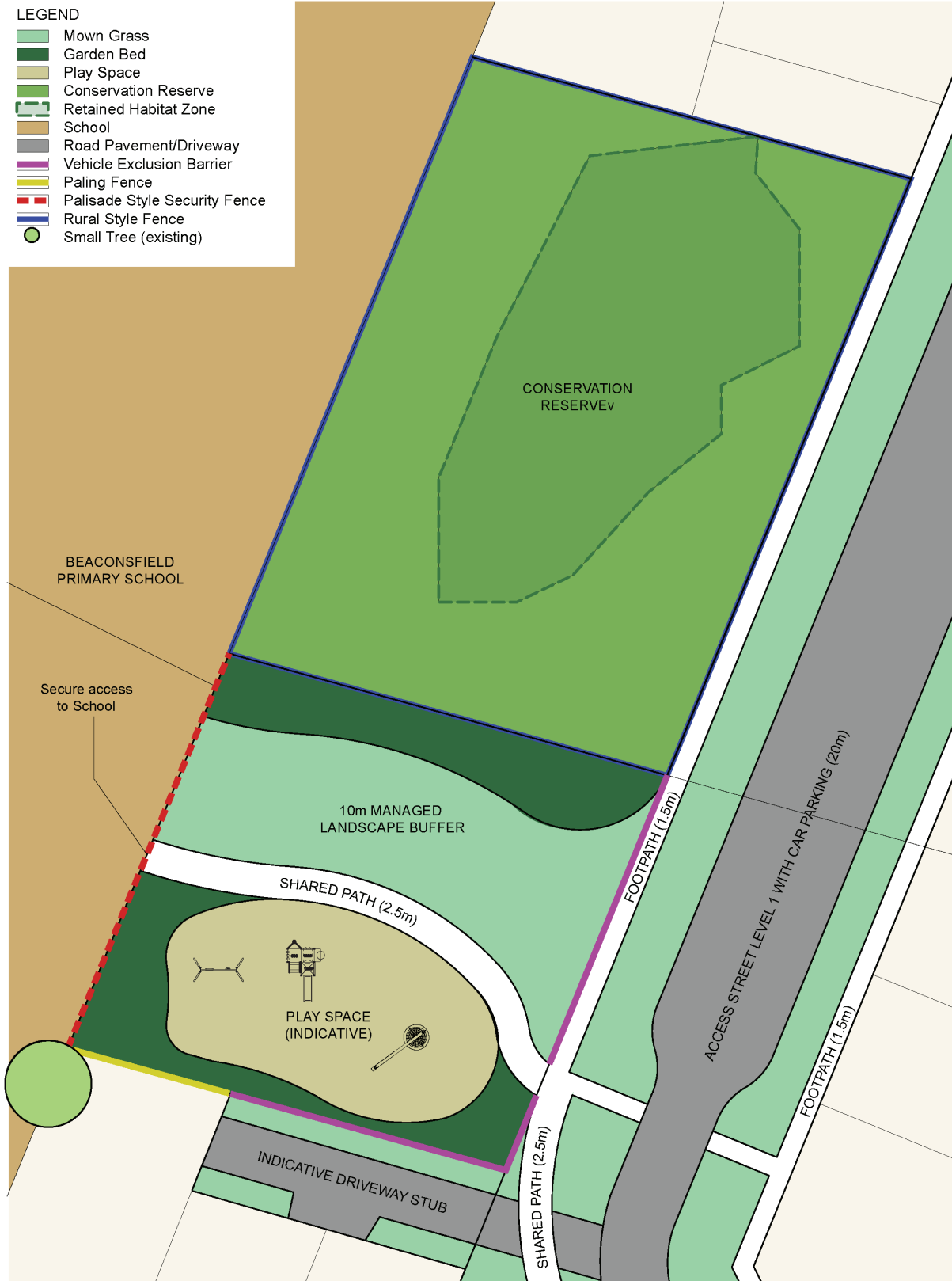


Figure 37 - Concept Plan - Local Open Space

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Figure 38 - Palisade style fence example

10.3. Bushfire Risk Assessments

Although the site is not located within a Bushfire Management Overlay or designated Bushfire Prone Area, fire risk should still be considered. The selection of appropriate plant species, preferably those with fire-retardant properties (refer to CFA’s Landscaping for Bushfire guide) and the provision of defensible space setbacks are recommended where appropriate.

10.4. Streetscape

Streetscape design within the GRADP should protect and enhance the “Bushland” landscape character, generally comprising canopy-forming Eucalyptus species. Streetscape treatments should contribute to visual cohesion, reinforce the natural character of the area, and integrate with the topography and private landscaping.

All streetscape planting must:

- Be designed to complement the existing vegetation patterns and maintain important sightlines to ridgelines and valleys.
- Use species listed in Table 26, with alternative species considered only in accordance with Cardinia Shire Council Developer Landscape Guidelines.
- Comply with Clause 52.37 – Tree Canopy of the Cardinia Planning Scheme, ensuring adequate canopy coverage and biodiversity outcomes.
- Incorporate sustainable design principles, including consideration of water availability, soil conditions, and climate resilience.
- Be coordinated with road infrastructure, street lighting, and utility services to minimise conflicts and ensure long-term maintenance.

Table 26 - Streetscape Tree Details

Scientific Name	Common Name	Mature Height (m)	Mature Spread (m)	CFA Flammability Analysis
<i>Eucalyptus melliodora</i>	Yellow Box	10 – 25	8 – 15	Preferred Street Tree: Low Flammability Bark (Box Bark)
Excellent shade tree with a broad, dense crown. Highly valued for honey production and provides habitat for native birds. Drought tolerant once established.				
<i>Eucalyptus camphora</i> ssp. <i>humeana</i>	Mountain Swamp Gum	8 – 15	6 – 10	Conditional Use: Moderate Flammability Bark. Requires strict ground litter removal.
Smaller gum with a dense, spreading canopy. Suitable for wetter soils and ideal for planting in broad verges or near drainage lines/retarding basins.				
<i>Eucalyptus rubida</i>	Candlebark	15 – 30	8 – 12	High Risk: Ribbon-shedding bark is highly flammable and acts as ladder fuel. Restricted to large park-edge planting only.
Stunning smooth white bark. Attractive weeping foliage. Fast-growing and provides light shade, ideal for streets where some winter sun penetration is desired.				
<i>Acacia dealbata</i>	Silver Wattle	10 – 25	6 – 10	Conditional Use: Fast growth leads to accumulation of fine, dry litter. Requires frequent pruning and non-flammable mulch.
Fast-growing, ornamental tree with feathery, silvery-blue foliage and masses of fragrant yellow flowers in late winter/spring. Excellent secondary or habitat tree.				
<i>Eucalyptus viminalis</i>	Manna Gum	20 – 40	10 – 15	High Risk: Sheds large, flammable ribbons of bark. Restricted use; requires constant removal of bark ribbons during fire season.
Iconic local tree with shedding bark and handsome spreading form. Requires deep, moist soil and sufficient space. Good for specimen or park-edge planting.				
<i>Acacia melanoxylon</i>	Blackwood	8 – 20	6 – 12	Preferred Street Tree: Generally tight bark and produces less volatile litter than many eucalypts.
Dense canopy tree with glossy, dark green phyllodes. Excellent screening and shade tree. Tolerant of moderate moisture variation, suitable for streetscapes.				
<i>Allocasuarina littoralis</i>	Black Sheoak	8 – 15	6 – 8	Conditional Use: Fine, needle-like foliage can accumulate dry litter, increasing flammability risk. Requires routine maintenance/litter removal.
Fine, dark foliage provides vertical texture and rustling sound. Very hardy, slow-growing, and adaptable to poorer soils. Provides habitat for Black Cockatoos.				
<i>Eucalyptus ovata</i>	Swamp Gum	10 – 20	8 – 15	Conditional Use: Likely sheds flammable ribbons of bark seasonally. Use limited to wetter, low-lying areas with low exposure risk.
Tolerant of heavy, periodically waterlogged clay soils, making it excellent for lower areas, swales, and raingardens in urban settings. Offers dense canopy for shade.				

11. Integrated Water Management

11.1. Background

The Glismann Road precinct forms part of Melbourne Water’s O’Neil Road Development Services Scheme (DS), situated within the Western Port catchment.

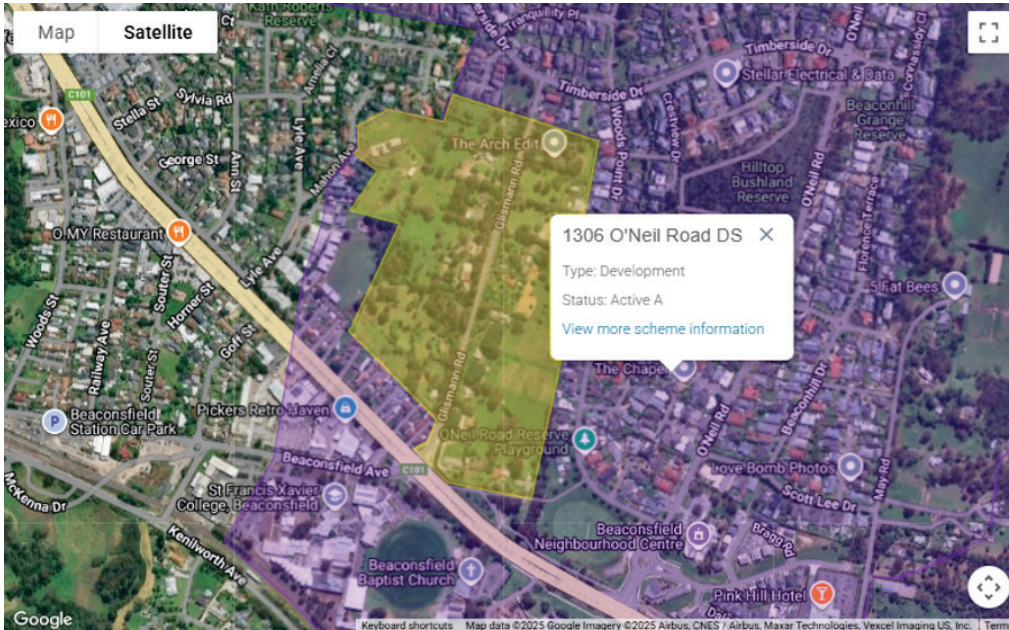


Figure 39 - Drainage Scheme map with the Glismann Road Development Plan area highlighted in yellow - source Melbourne Water website.

The landform naturally drains from north to south, with pronounced high points along the eastern ridgeline (approximately 83 m AHD) and the north-west corner (approximately 75 m AHD), creating a central valley that conveys overland flows towards Princes Highway. The lowest point, at around 49 m AHD, represents the discharge location where stormwater enters Melbourne Water’s trunk drainage network via the existing 1800 mm and twin 1500 mm culverts beneath the highway.

A comprehensive drainage management strategy prepared by Water Technology (2014 and 2016) outlines the stormwater infrastructure, flood mitigation, and water quality management requirements for the precinct. The original 2014 strategy proposed an integrated system comprising a retarding basin and wetland located within the triangular parcel of open space between the Old Princes Highway and Property 17, 18, 19 and 20. This system was designed to attenuate post-development peak flows to pre-development levels and achieve best-practice pollutant reduction outcomes in accordance with the Urban Stormwater Best Practice Environmental Management Guidelines – namely, 80% reduction in total suspended solids (TSS), 45% reduction in total phosphorus (TP), and 45% reduction in total nitrogen (TN).

Following further investigations, the 2016 Water Technology assessment recommended replacing the retarding basin with a flood protection levee, a measure subsequently supported by Melbourne Water. This revised approach involves the construction of a 450 mm high levee bank along the frontage of Property 17, 18, 19 and 20 to protect existing dwellings from below-floor flooding.

The levee is to follow the existing shared path within Crown land, maintain access to the affected properties, and extend eastwards to cross the table drain - incorporating a drainage structure to allow upstream catchment flows to discharge appropriately. The report also identifies options to address stormwater quality obligations via stormwater quality offset contributions, where on-site treatment is not provided.

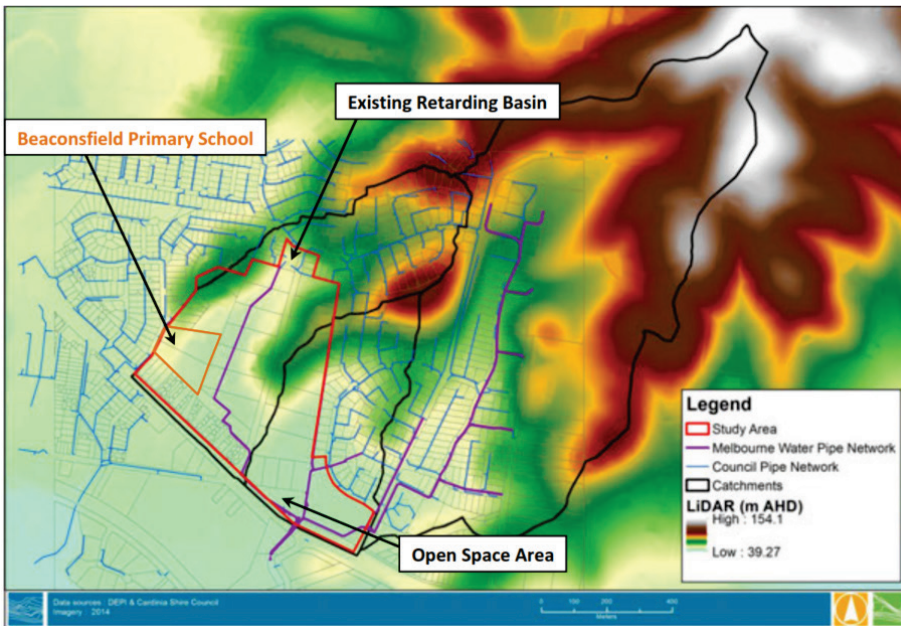


Figure 40 - Catchment Map and Drainage Network plan. Source: Glismann Road Drainage Scheme Report prepared by Water Technology , July 2014.



Figure 3-3 Alignment Option 1 and 1% AEP Modelled Flood Depths

Figure 41 - Flood Levee plan with post developed flood modelling overlaid. Source: Glismann Road Drainage Scheme Report prepared by Water Technology, May 2016.

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11.2. Proposed Drainage system

The internal drainage network will comprise a pit and pipe system designed to accommodate the 1 in 5-year rainfall event, with overland flow paths and the surrounding road network safely conveying the 1% AEP (100-year ARI) event. Minimum freeboard requirements of 300 mm above the 1% AEP flood level for drainage infrastructure and 600 mm for waterways will apply to future development, consistent with Melbourne Water and Council standards. The overall drainage design will need to be developed in accordance with contemporary best-practice water quality and flood management standards.

Melbourne Water's pre-development advice (May 2025) confirms that permanent drainage works are required within the precinct under the O'Neil Road DS and that a drainage agreement under the Water Act 1989 must be executed prior to Statement of Compliance. This agreement will formalise drainage contributions currently set at \$233,237 per hectare, comprising a \$123,924/ha hydraulic charge, a \$74,889/ha stormwater quality charge, and a \$34,424/ha gross water quality offset.

Melbourne Water also requires the submission and approval of a Stormwater Management Strategy demonstrating compliance with the intent of the O'Neil Road DS, supported by functional drainage design, flood modelling, and WSUD integration with any planning permit application for subdivision. Interim sediment control and temporary retardation measures must be implemented to manage downstream impacts until permanent assets are operational.

Consistent with the requirements of DPO19, proposals to subdivide land must incorporate an integrated water management approach that responds to the recommendations of the Water Technology (2014 and 2016) studies. Collectively, these measures will ensure the GRADP is supported by a coordinated and sustainable drainage system that aligns with Melbourne Water's regional drainage strategy, provides appropriate flood protection, achieves best-practice water quality outcomes, and satisfies the integrated water management and servicing objectives of DPO19.



Node Ref.	Length(m)	Diameter (mm)	Comments/ Ownership
L1 - B11	90	300	Council Owned
M1 - B11	90	300	Council Owned
K1 - B9	40	450	Council Owned
P1 - C1	45	375	Council Owned
L2 - L1	30	300	Council Owned
N1 - N1A	25	300	Council Owned
N1A - B12	60	300	Council Owned
P2 - P1	45	300	Council Owned

Figure 42 - Plan showing the existing Melbourne Water underground scheme drains in dark blue and proposed drainage pipes blue and dashed red lines. Source: Melbourne Water Predevelopment Advice.

11.2.1. Levee Bank

As outlined in Section 11.1, a levee bank is required as part of the O’Neil Road Development Services Scheme (DS).

Preliminary discussions with all relevant referral and Responsible Authorities, including Melbourne Water, the DTP, Cardinia Shire Council, and the Responsible Authority for the Crown land are essential at the very early stages of the planning process. Any Stormwater Management Strategy submitted with a planning permit application to subdivide land must be developed in close consultation with these authorities.

This engagement must address not only whether the proposed subdivision triggers the need for the levee bank and the timing of its delivery, but also the standards and management of its construction, long-term maintenance, and implementation responsibilities.

Importantly, the proposed drainage solution must satisfy the specific requirements of the Responsible Authority for the Crown land on which the levee is to be located.

The approach should provide clear direction while maintaining a high level of flexibility for the main parties involved, unless otherwise agreed by the relevant Responsible Authority.

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12. Utilities

Utilities services are required to be provided and extended as part of subdivision works within the GRADP area. Preliminary advice from utility providers is provided below, confirming the availability of all services in proximity to the site. Applications for planning permit for subdivision are to provide advice specific to the subdivision, confirming the ability of services to the site.

12.1. Sewer

South-East Water (SEW) has provided the following preliminary servicing advice for development within the Glismann Road precinct:

‘There are capacity constraints in the surrounding sewer network. Whilst there is some spare capacity in the existing sewers surrounding the site, to service the entire development will require upgrades/ duplication of the existing system and downstream pump station located in Adamson Road. The scope of upgrade works will be influenced by the reticulated sewer design for the development proposal.

The majority of the site will need to be serviced by the 225mm sewer main to the south, with the remainder discharging to the 150mm sewer main to the north, as indicated on the attached ‘Sewer Indicative Alignment Plan’. Discharge to the northern sewer should be maximised as there are fewer capacity constraints in this system.’

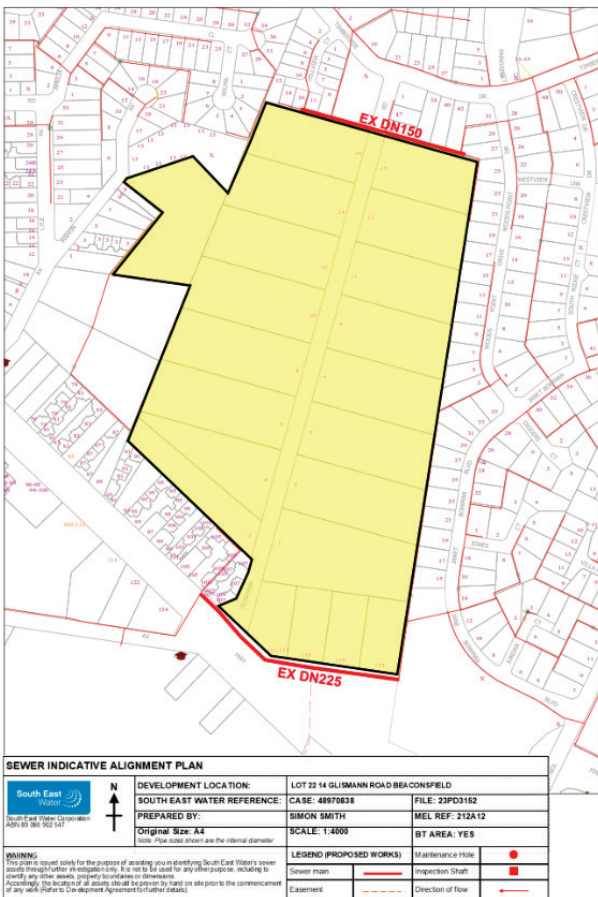


Figure 43 - Existing sewer (South East Water)

Developers will be responsible for the construction of sewer reticulation required for each development. Details of the sewer reticulation and extension/connection works will be confirmed by a formal SEW Development Deed.

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12.2. Potable Water:

South-East Water (SEW) has provided the following preliminary servicing advice for development within the Glismann Road precinct:

'To provide drinking water facilities to the development, it will be necessary to remove the existing 100mm AC water mains located in Glismann Road and Princes Highway and construct an estimated 910m of 150mm water main as shown A-B on the attached 'Drinking Water Indicative Alignment Plan'.

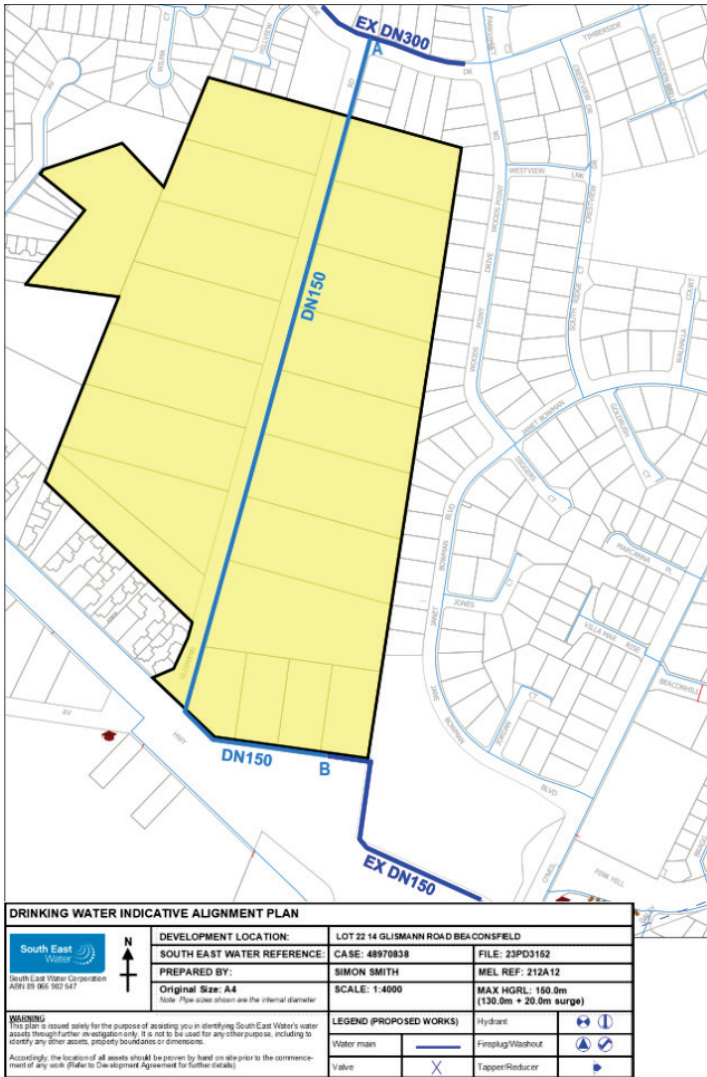


Figure 44 - Existing potable water (South East Water)

Developers will be responsible for the construction of water reticulation required for each development. Details of the water reticulation and extension/connection works will be confirmed by a formal SEW Development Deed.

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12.3. Electrical

Ausnet is the responsible authority for the provision of electricity supply to the development plan area.

The Ausnet Services BYDA (Before You Dig Australia) plan below shows existing 22kV distribution overhead powerlines along Princes Highway and extending halfway up Glismann Road.

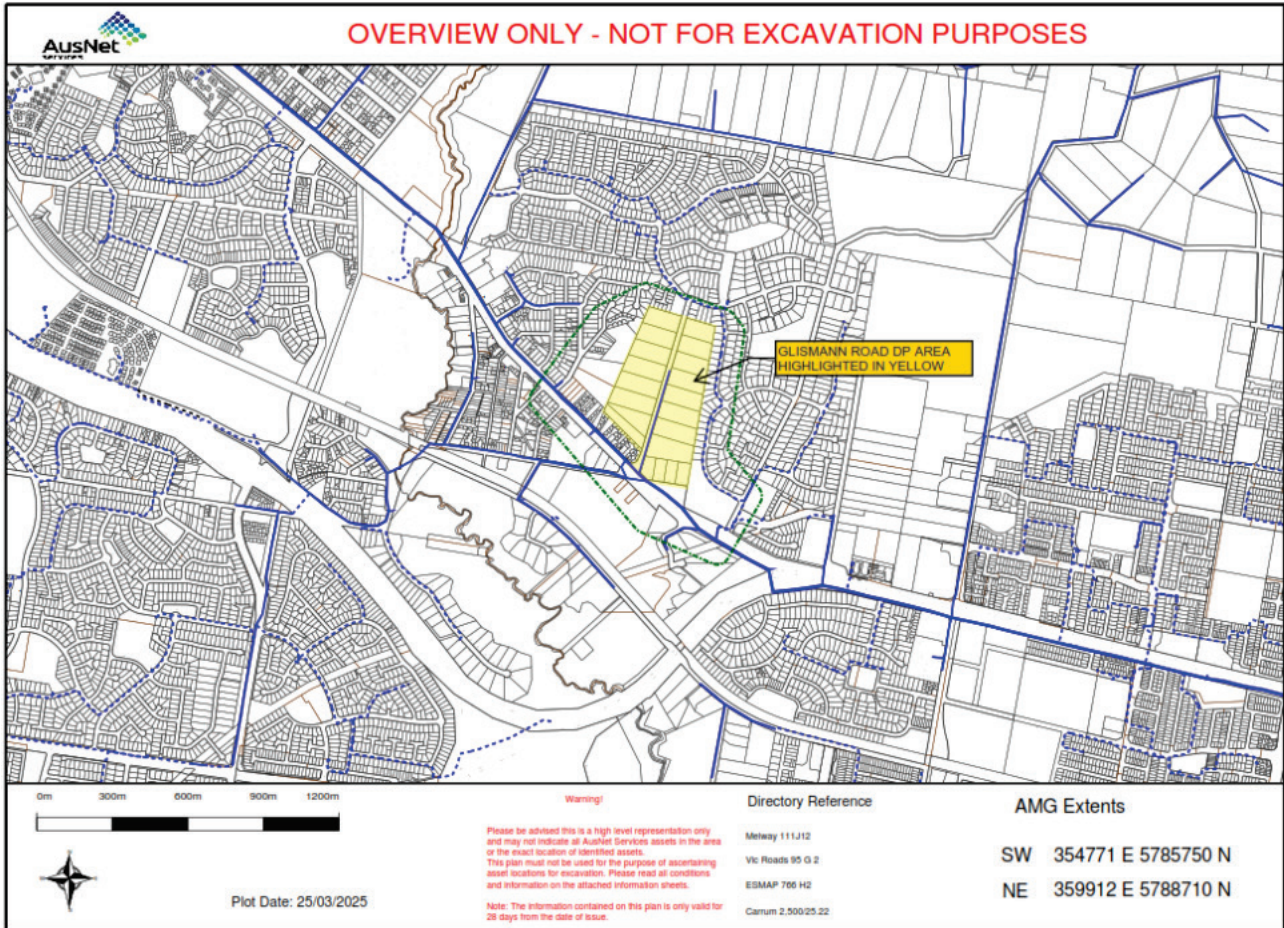


Figure 45 - Existing electrical network (Ausnet)

The proposed development of the Glismann Road area will be required to connect into and extend the electrical infrastructure as per responsible authority requirements. Details of the requirements for supply will be subject to receipt of formal offer from the relevant electricity authority at the time of development and detailed electrical design.

Developers are responsible for the construction of electricity infrastructure to service their development.

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12.4. Telecommunications

A BYDA (Before You Dig Australia) investigation confirms existing NBN (National Broadband Network) infrastructure along Old Princes Highway and Glismann Road. Developers will be required to install pit and pipe infrastructure suitable for optical fibre installation and enter into a Master Development Agreement with NBN or other communications provider.

12.5. Gas

As per Clause 53.03 of the Cardinia Planning Scheme, no new lots created by subdivision, or new dwellings, can be connected to reticulated gas services.

13. Preliminary Risk Screening Site Assessments

In accordance with DPO19, a Preliminary Risk Screening Assessment (PRSA) must be prepared for specified properties in the GRADP. The PRSA must accompany any planning permit application to subdivide land and completed by a suitably qualified consultant.

The following properties require a PRSA:

- Property 1 (2 Glismann Road).
- Property 4 (8 Glismann Road).
- Property 5 (10 Glismann Road).
- Property 16 (1 Glismann Road).

If a PRSA deems an Environmental Audit is required for a property, the Audit must be completed prior to the commencement of construction or carrying out intrusive works (other than site investigation works to determine the level of contamination if any on the land) at that property.

14. Development Coordination

Development within the GRADP will occur progressively over time and will be influenced by a range of factors, including:

- The aspirations and timing of individual landowners.
- The fragmented pattern of land ownership within the GRADP.
- Accessibility to existing infrastructure, and the cost of extending new services.
- The obligations and requirements of DCP05 and DPO19 for road and infrastructure delivery.

As outlined in Section 5.1.1, delivery of Glismann Road is the key enabler of development across the GRADP:

- RD-01 facilitate development in the southern part of the GRADP.
- RD-02, in combination with RD-01 and TM-01 facilitates development in the northern part of the GRADP.

Preliminary collaboration between landowners is strongly encouraged to support coordinated subdivision and the shared delivery of infrastructure identified in DCP05, DPO19, and the GRADP. However, this is not solely about meeting the requirements of planning controls, it is also about ensuring the timely and integrated delivery of other essential services, including utilities, drainage, and other infrastructure. These requirements often involve multiple authorities and service providers, and must be considered from the outset.

The challenges associated with fragmented land ownership are not unique to Glismann Road. In similar contexts, the staging of development can be delayed or complicated when infrastructure delivery depends on multiple parties. Early and proactive collaboration between landowners can help reduce delays, avoid duplication, and ensure that infrastructure is delivered in a logical, cost-effective, and coordinated manner that meets the expectations of all relevant authorities.

The Development Coordination Plan (Figure 46) identifies opportunities where cooperative delivery between landowners could assist in unlocking development through coordinated infrastructure provision. This is a guide only.

14.1. Specific Delivery Requirements under DP019

The following requirements must be addressed when preparing subdivision or development applications within the GRADP:

- A FLP **must** be prepared for the entire length of Glismann Road (RD-02), or for a length as agreed by the Responsible Authority, to accompany the first subdivision requiring access to Glismann Road (RD-02) or TM-01. An allowance for a FLP is included in DCP05. The FLP must include TM-01. Refer to Section 5.1.1 for further details.
- The first subdivision application that includes land designated for public open space (either Property 3 or Property 4) must be accompanied by an indicative concept master plan for the entire park area. This plan must demonstrate how the open space will be delivered and developed in stages to ensure each stage provides a functional outcome.
- Prior to the issue of a Statement of Compliance for any subdivision, the developer must complete all works identified in the property's SMP that are necessary to stabilise land and ensure safe and sustainable development.
- All subdivision applications must be accompanied by a site-specific staging plan that considers surrounding approvals and the delivery of required items.
- Where a subdivision includes land affected by RD-03, RD-04 or RD-05, or the additional internal streets described at Section 0, the staging plan must demonstrate:
 - The delivery of the access street connections in the first stage of development.
 - That the access street is constructed to the title boundary of the adjoining property.
- An application to subdivide land must, in consultation with Melbourne Water, DTP, the Responsible Authority for the crown land and Cardinia Shire Council address the timing of the delivery of the levee bank, unless otherwise agreed by the Responsible Authority. Refer to further discussion at Section 0.
- Where a PRSA deems an Environmental Audit to be necessary for any given property, the Audit must be completed prior to the commencement of construction or carrying out intrusive works (other than site investigation works to determine the level of contamination if any on the land) at that property. Refer to further discussion at Section 13.

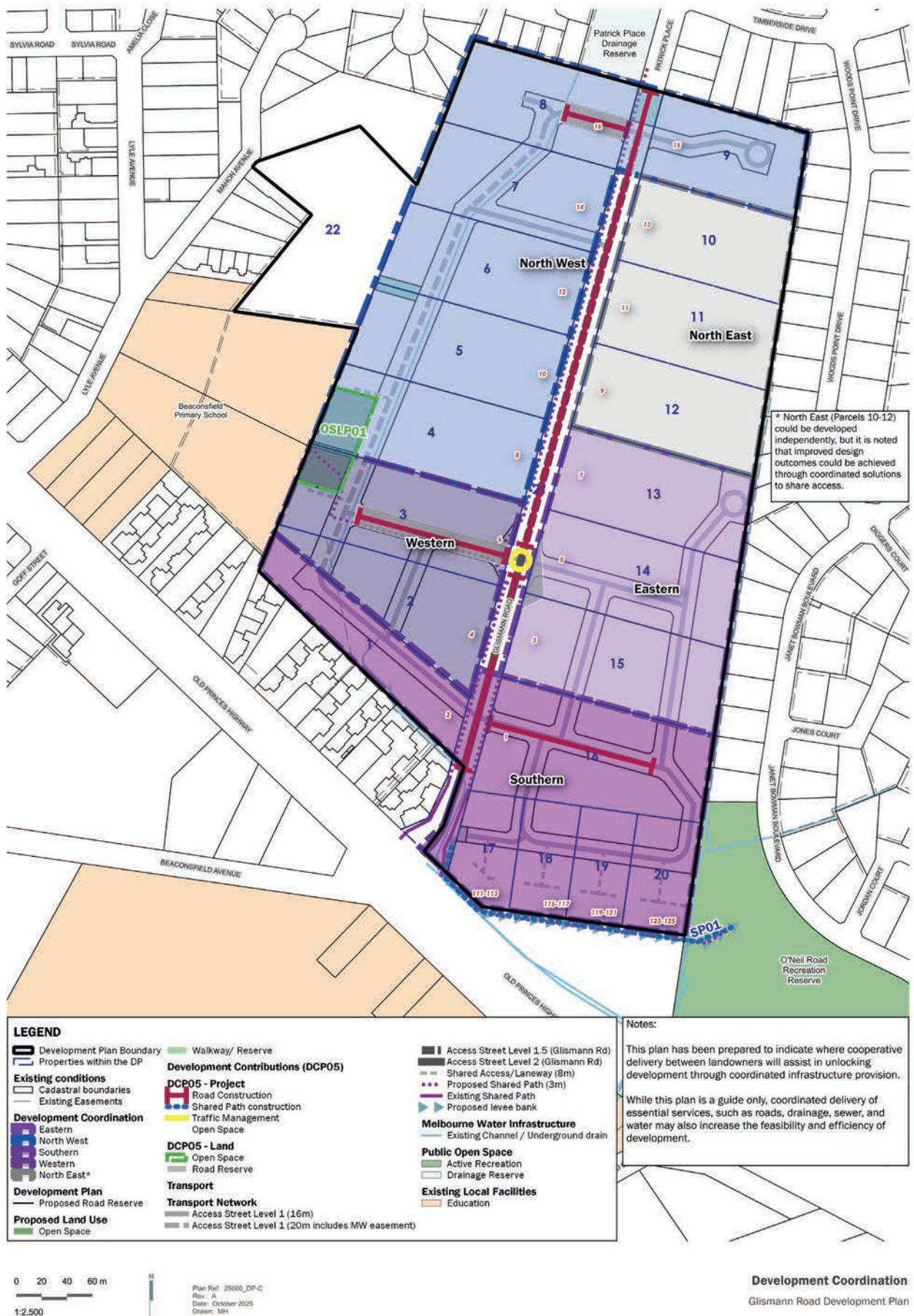


Figure 46 - Development Coordination Plan

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14.2. Delivery of Glismann Road

- The first application to subdivide land with access to Glismann Road (Access Street Level 1.5) (which is RD-02) must include a FLP for the entire length of the road (RD-02), or a length to the satisfaction of the Responsible Authority.
- Construction of Glismann Road must occur continuously from the Old Princes Highway, with each subdividing property responsible for delivering its full road frontage. This ensures the road remains a continuous and functional connection as development progresses.
- As subdivision applications approach the junction of Glismann Road where RD-01, TM-01, and RD-02 meet, the Responsible Authority may require that the application for subdivision be accompanied by a FLP for the full length of RD-02, TM-01, and a portion of RD-01, or a length of road to the satisfaction of the Authority.
- At a minimum, a FLP must be prepared for the first subdivision requiring access to RD-02. This FLP must include TM-01 (refer to Table 10 for further information). The FLP must be prepared and approved for the full length of RD-02, or a length as agreed by the Responsible Authority. The FLP must also address Property 3 and Property 14, which require TM-01 to provide site access, unless otherwise agreed by the Responsible Authority.

Refer to Section 5.1.1 for further details.

15. Land Budget

The land budget provided in **Error! Reference source not found.** is an extract of the Land Budget Summary in the DCP (Table 1 on page 5).

Table 27 DCP Land Budget Summary

LAND USE BUDGET	HECTARES	% OF DP AREA
TOTAL DEVELOPMENT PLAN AREA	19.69	100%
ENCUMBERED LAND		
Significant area of slope over 20%	1.77	9.0%
Sub-total Encumbered	1.77	9.0%
TRANSPORT		
Existing Road Reserves (retained for road purposes)	1.33	6.8%
Road reserve land included in DCP	0.57	2.9%
Sub-total Transport	1.90	9.7%
CREDITED OPEN SPACE		
Local Park	0.30	1.5%
Sub-total Credited Open Space	0.30	1.5%
NET DEVELOPABLE AREA (NDA) HA	15.72	79.8%
Residential NDA	15.72	79.8%
Commercial/Other NDA	0.00	0.0%
Estimated Residential lot/dwelling yield	267 dwellings	

Source: Glismann Road Development Plan, Urban Design and Management.

As noted in Section 1.3, Property Reference 22, which is 11 Mahon Avenue is not included in the Development Contributions Plan but is included in the Development Plan. For further information regarding the exclusion of Property Reference 22 can be found in the [Panel Report for Cardinia Planning Scheme Amendment C238card](#). Therefore, Property 22 has not been included in the results in Table 27.

Content in Table 28, is an extract of Appendix A of the DCP (page 20 – 21) which is a detailed land budget. This table identifies the property specific land use budget for each parcel based on the DCP area.

Table 28 - Property Specific Land Use Budget

PROPERTY ID	LAND DESCRIPTION	TOTAL AREA (HECTARES)	TRANSPORT		CREDITED OPEN SPACE	ENCUMBERED LAND	NET DEVELOPABLE AREA (HECTARES)	NET DEVELOPABLE AREA % OF PROPERTY	LOCAL ROADS	NET RESIDENTIAL AREA (NRA)	OPEN SPACE
			Existing Road Reserves	Road reserve land included in DCP	Local Park	Significant area of slope over 20%			Access Street		% of NDA
PRIVATE LAND											
1	2 Glismann Road	0.77	0.00	0.00	0.00	0.00	0.77	100%	0.00	0.77	0%
2	4 Glismann Road	0.90	0.00	0.00	0.00	0.00	0.90	100%	0.00	0.90	0%
3	6 Glismann Road	1.23	0.00	0.20	0.12	0.00	0.91	74%	0.14	0.77	13%
4	8 Glismann Road	1.17	0.00	0.00	0.18	0.11	0.88	75%	0.14	0.74	21%
5	10 Glismann Road	1.12	0.00	0.00	0.00	0.24	0.88	79%	0.14	0.74	0%
6	12 Glismann Road	1.06	0.00	0.00	0.00	0.00	1.06	100%	0.14	0.92	0%
7	14 Glismann Road	1.01	0.00	0.00	0.00	0.12	0.89	88%	0.16	0.73	0%
8	16 Glismann Road	0.95	0.00	0.09	0.00	0.26	0.61	64%	0.03	0.58	0%
9	15 Glismann Road	0.84	0.00	0.00	0.00	0.00	0.84	100%	0.00	0.84	0%
10	13 Glismann Road	0.89	0.00	0.00	0.00	0.36	0.53	60%	0.00	0.53	0%
11	11 Glismann Road	0.94	0.00	0.00	0.00	0.23	0.71	76%	0.00	0.71	0%
12	9 Glismann Road	1.00	0.00	0.00	0.00	0.33	0.67	67%	0.00	0.67	0%
13	7 Glismann Road	1.05	0.00	0.00	0.00	0.12	0.93	89%	0.07	0.86	0%
14	5 Glismann Road	1.11	0.00	0.04	0.00	0.00	1.07	97%	0.11	0.96	0%
15	3 Glismann Road	1.17	0.00	0.01	0.00	0.00	1.16	99%	0.11	1.05	0%
16	1 Glismann Road	1.47	0.00	0.24	0.00	0.00	1.23	100%	0.12	1.11	0%
17	111-113 Old Princes Hwy	0.41	0.00	0.00	0.00	0.00	0.41	100%	0.00	0.41	0%

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18	115-117 Old Princes Hwy	0.41	0.00	0.00	0.00	0.00	0.41	100%	0.00	0.41	0%
19	119-121 Old Princes Hwy	0.41	0.00	0.00	0.00	0.00	0.41	100%	0.00	0.41	0%
20	123-125 Princes Old Hwy	0.45	0.00	0.00	0.00	0.00	0.45	100%	0.00	0.45	0%
Sub total		18.36	0.00	0.57	0.30	1.77	15.72	86%	1.16	14.56	2%
Public Land											
21-R	Glismann Road reserve	1.33	1.33	0.00	0.00	0.00	0.00	0%	0.00	0.00	
SUB-TOTAL		1.33	1.33	0.00	0.00	0.00	0.00	0%	0.00	0.00	
TOTALS		19.69	1.33	0.57	0.30	1.77	15.72	80%	1.16	14.56	2%

16. Individual Property Comments

Under DPO19, specific documentation must accompany all proposals to subdivide land, as relevant to the Property. Where the Responsible Authority considers that an application requirement is not relevant to the assessment of an application, the requirement may be waived or reduced. Applicants should refer to DPO19 for full detail of the applicable requirements.

Each subdivision application must include, as applicable:

- An assessment demonstrating how the proposal implements the vision, objectives, requirements and guidelines of the GRADP.
- An overall masterplan for all land in the same or contiguous ownership.
- A Transport Impact Assessment Report that responds to *Traffic works (May 2022) Glismann Road Residential Development, Beaconsfield, Traffic Impact Assessment Report* to the satisfaction of the relevant roads authority (DTP or Council).
- An Infrastructure Plan.
- A Stormwater Management Strategy that details the staging and timing of drainage works, including any temporary outfall provisions, to the satisfaction of Melbourne Water and the Responsible Authority.
- An assessment by a suitably qualified cultural heritage professional that addresses the recommendations of *Tardis Enterprises Pty Ltd (November 2010) The Glismann Road, Beaconsfield Structure Plan Cultural Heritage Management Plan (CHMP) 11452*.
- A flora and fauna assessment that responds to *Ecology Partners Pty Ltd (October 2010) Biodiversity Assessment for Area 1, 'Beaconsfield', Beaconsfield, Victoria*.
- A Native Vegetation Information Management (NVIM) report identifying any vegetation to be retained and associated protection measures.
- A Slope Management Plan be prepared by a suitably qualified person, generally in accordance with the Slope Management Guidelines approved as part of the GRADP.
- Building design guidelines and fencing controls addressing the GRADP housing planning and design guidelines.
- A Landscape Masterplan demonstrating how the development responds GRADP, including key themes, landscape principles and the intended subdivision and built character.

Additional guidance for each property, **where development is proposed**, is provided overleaf. This guidance is indicative only and does not represent an exhaustive or prescriptive set of requirements. It must be read in conjunction with the broader content of the GRADP, DPO19, DCPO5, and the relevant provisions of the Cardinia Planning Scheme. This material is intended to support early discussions and decision-making, but final outcomes will be subject to detailed assessment and approval by the relevant referral authorities and the Responsible Authorities.

16.1. Property 1 – 2 Glismann Road



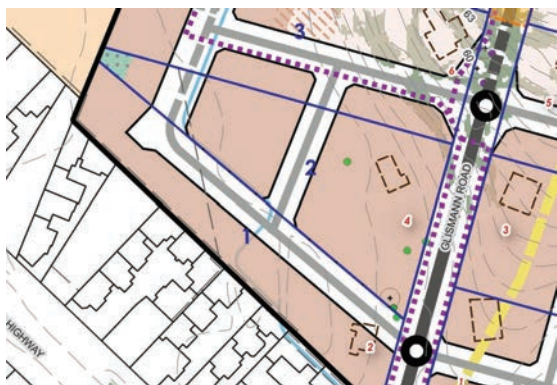
REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Local Access Street (16m wide) along the northern boundary of Property 1, vested in the Responsible Authority. • Glismann Road (RD-01) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. • The intersection with RD-01 must be designed to accommodate a future four-way roundabout with RD-05. • The roundabout is to be constructed when RD-05 is constructed, unless otherwise agreed by the Responsible Authority. • Design, construction and land costs for the roundabout are to be met by Property 1 and secured via a Section 173 Agreement as a condition of any permit for subdivision or development. • Shared Path to be constructed on Glismann Road. • Provide connections to Property 2. • Refer to Table 11 for further information.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Section 7 as relevant.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.

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Vegetation - Section 9	<ul style="list-style-type: none"> • Consider retention of Small Trees located on northern boundary. • Consider protection of MSA identified tree on Property 2. • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • PRSA required. Refer to Section 13.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Southern coordination area.

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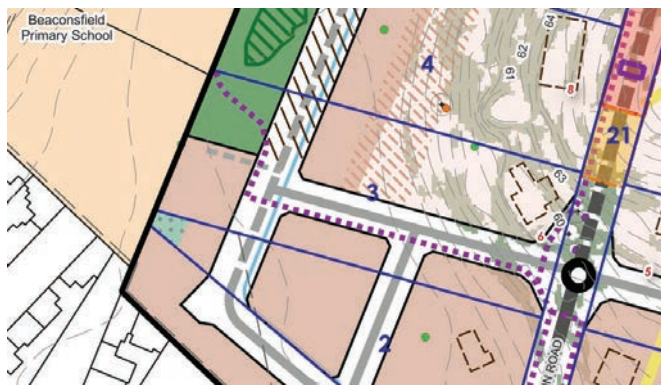
16.2. Property 2 – 4 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 1 or Property 3. • Glismann Road (RD-01) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. • Shared Path to be constructed on Glismann Road. • Provide connections to Property 1 and Property 3.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential. • Native Vegetation Offset Planting site retained.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Section 7 as relevant.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.
Vegetation - Section 9	<ul style="list-style-type: none"> • Consider retention of Small Trees. • Consider retention and protection of MSA identified tree. • Native vegetation offset planting area identified. May be relocated if developed subject to relevant provisions.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Western coordination area.

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16.3. Property 3 – 6 Glismann Road

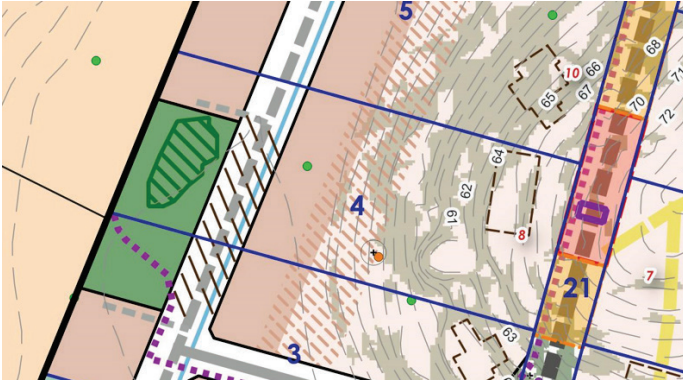


REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street RD-03 and DCP project LA-01. If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. Glismann Road (RD-02) must be delivered for the extent of the Property frontage. The intersection of RD-03 with RD-01 must be designed to provide TM-01. There is no allowance in the DCP for the splays within Property 3 required for TM-01. Provide connections to Property 2 and Property 4. Shared Path to be constructed on Glismann Road and RD-03. Refer to Table 7 for further information. Refer to Table 10 for further information. Potential 'Restricted Vehicle Access' restrictions – refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential and Visually Sensitive Residential areas. Local Open Space (LA-05).
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered. Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> Consider retention of Small Tree. Consider retention and protection of MSA identified trees. Respond to area of High Overall Landscape Values. Refer to Section 9.3.

Landscape - Section 10	<ul style="list-style-type: none"> • Provide direct road frontage to LA-05. On-street parking to be provided both sides of carriageway adjacent to LA-05. • The first application to subdivide land containing LA-05 must be accompanied by an indicative concept master plan for the entire local park. • This plan must demonstrate how LA-05 will be delivered and developed in stages, ensuring that each stage provides a functional and accessible open space outcome for each of Property 3 and Property 4. • The delivery of LA-05 should not be dependent on the subdivision or development of all the LA-05 land. • Refer to Table 24 and Section 10.2.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Western coordination area.

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16.4. Property 4 – 8 Glismann Road

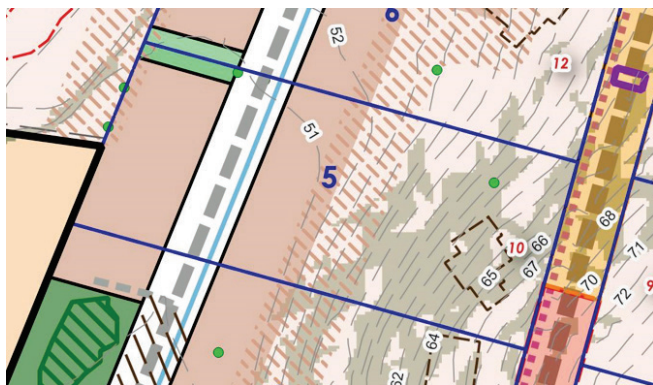


REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 3 or Property 5. • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • Provide connections to Property 3 and Property 5. • Shared Path to be constructed on Glismann Road. • ‘No Vehicle Access Permitted’ and ‘Restricted Vehicle Access’ restrictions apply to Glismann Road frontage. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential and Visually Sensitive Residential areas. • Local Open Space (LA-05).
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Large Old Tree to be retained within subdivision layout. Refer to Section 9.2. • Consider retention of Small Tree. • Respond to area of High Overall Landscape Values. Refer to Section 9.3.

Landscape - Section 10	<ul style="list-style-type: none"> • Provide direct road frontage to LA-05. On-street parking to be provided both sides of carriageway adjacent to LA-05. • The first application to subdivide land containing LA-05 must be accompanied by an indicative concept master plan for the entire local park. • This plan must demonstrate how LA-05 will be delivered and developed in stages, ensuring that each stage provides a functional and accessible open space outcome for each of Property 3 and Property 4. • The delivery of LA-05 should not be dependent on the subdivision or development of all the LA-05 land. • Conservation Zone within LA-05 is to be retained. • Refer to Table 24 and Section 10.2.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • PRSA required. Refer to Section 13.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North West coordination area.

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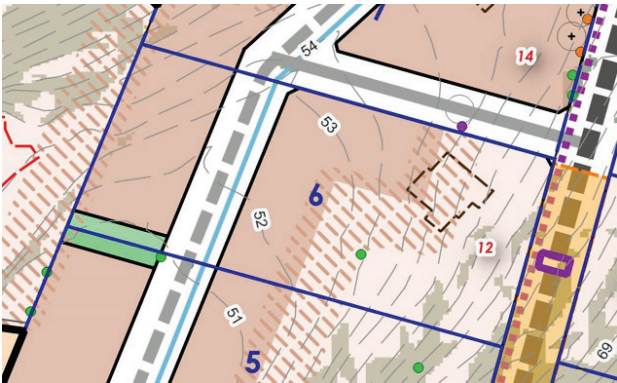
16.5. Property 5 – 10 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 4 or Property 6. • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • Provide connections to Property 4, Property 6 and a pedestrian connection to Property 22. • Shared Path to be constructed on Glismann Road. • 'No Vehicle Access Permitted' and 'Restricted Vehicle Access' restrictions apply to Glismann Road frontage. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Consider retention of Small Tree. • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • PRSA required. Refer to Section 13.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North West coordination area.

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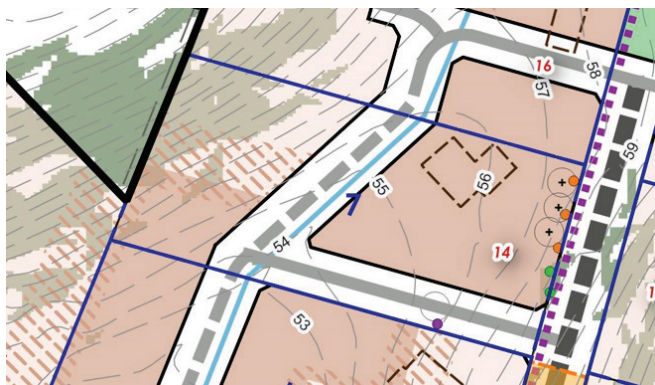
16.6. Property 6 – 12 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 5 or Property 7. • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • Provide connections to Property 5, Property 7 and a pedestrian connection to Property 22. • Shared Path to be constructed on Glismann Road. • 'Restricted Vehicle Access' restrictions. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Small areas included in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Consider retention of Small Tree. • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North West coordination area.

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16.7. Property 7 – 14 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street (16m wide) along the southern boundary of Property 7, vested in the Responsible Authority. If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. Glismann Road (RD-02) must be delivered for the extent of the Property frontage. Shared Path to be constructed on Glismann Road. Provide connections to Property 6 and 8. Refer to Table 12 for further information.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered. Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> Very Large Old Tree and Large Old Trees to be retained within subdivision layout. Refer to Section 9.2. Consider retention of Small Trees. Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> North West coordination area.

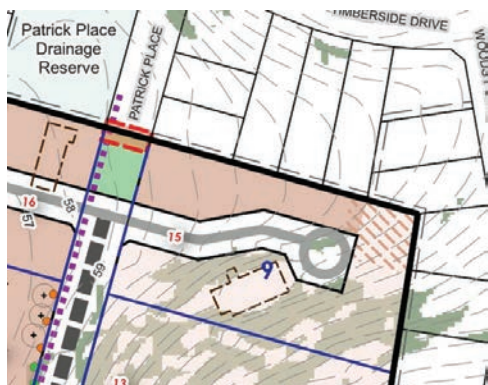
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16.8. Property 8 – 16 Glismann Road



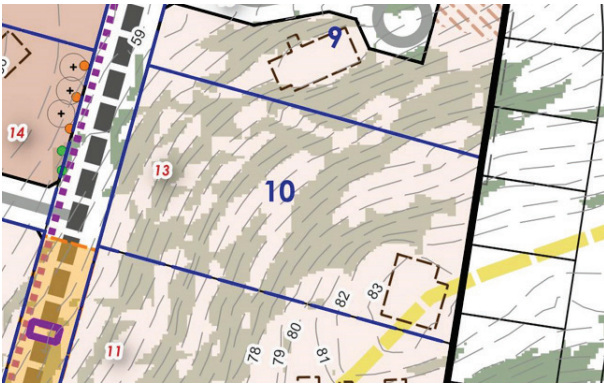
REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street RD-04 and DCP project LA-02. If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. Glismann Road (RD-02) must be delivered for the extent of the Property frontage. Provide connection to Property 8. Shared Path to be constructed on Glismann Road. Refer to Table 8 for further information.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered. Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> Consider interface to reserve at truncation of Glismann Road, and Patrick Place Drainage Reserve, including fencing treatment.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> Accommodation of existing Melbourne Water drain.
Utilities - Section 12	<ul style="list-style-type: none"> Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> North West coordination area.

16.9. Property 9 – 15 Glismann Road



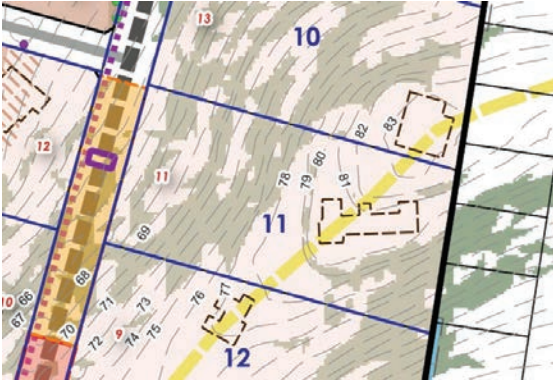
REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street (16m wide) to provide access, vested in the Responsible Authority. If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. Glismann Road (RD-02) must be delivered for the extent of the Property frontage.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered. Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> Consider interface to reserve at truncation of Glismann Road, including fencing treatment.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> North West coordination area.

16.10. Property 10 – 13 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Visually Sensitive Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North East coordination area.

16.11. Property 11 – 11 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • 'Restricted Vehicle Access' restrictions. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Visually Sensitive Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Visually sensitive area - refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North East coordination area.

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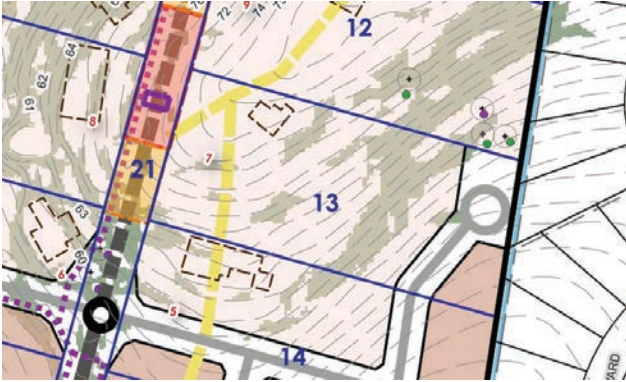
16.12. Property 12 – 9 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • ‘No Vehicle Access Permitted’ and ‘Restricted Vehicle Access’ restrictions apply to Glismann Road frontage. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Visually Sensitive Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Visually sensitive area - refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Very Large Old Tree to be retained within subdivision layout. Refer to Section 9.2. • Consider retention of Medium Trees. • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • North East coordination area.

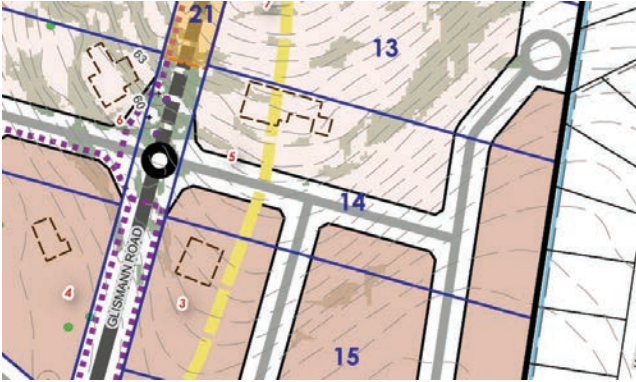
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16.13. Property 13 – 7 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 14. • If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. • Glismann Road (RD-02) must be delivered for the extent of the Property frontage. • 'No Vehicle Access Permitted' and 'Restricted Vehicle Access' restrictions apply to Glismann Road frontage. Refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Visually Sensitive Residential and Neighbourhood Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Visually sensitive area - refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Respond to area of High Overall Landscape Values. Refer to Section 9.3.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Eastern coordination area.

16.14. Property 14 – 5 Glismann Road



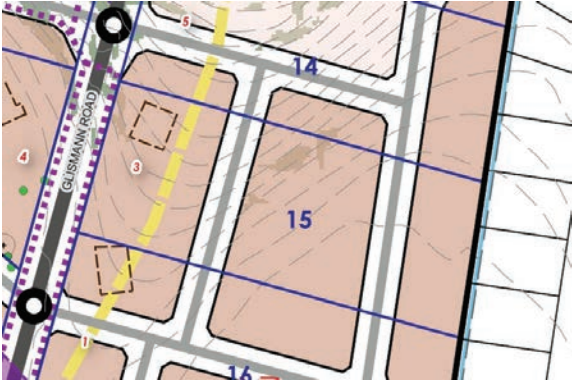
REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street (16m wide) to provide access, vested in the Responsible Authority. If not already constructed, TM-01 and Glismann Road (RD-01) must be concurrently constructed. Glismann Road (RD-02) must be delivered for the extent of the Property frontage. The intersection of the Local Access Street with RD-01 must be designed to provide TM-01 with access to RD-03. DCP Projects LA-04a and LA-04b fund the splays for TM-01 on the eastern side of Glismann Road. LA-04a allocates 115sqm for Property 15 (3 Glismann Road) and LA-04b allocates 366sqm for Property 14 (5 Glismann Road). There is no allowance in the DCP for splays, should they be required, on the western side of Glismann Road to accommodate TM-01. Provide connection to Property 13 and two connections to Property 15. Shared Path to be constructed on Glismann Road and RD-03. Refer to Table 7 for further information. Potential 'Restricted Vehicle Access' restrictions – refer to Section 5.5.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential and Visually Sensitive Residential areas.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Partly located in visually sensitive area. Refer to Section 0 regarding site access and siting of building envelopes.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered. Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> Refer to Section 10 as relevant.

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Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Eastern coordination area.

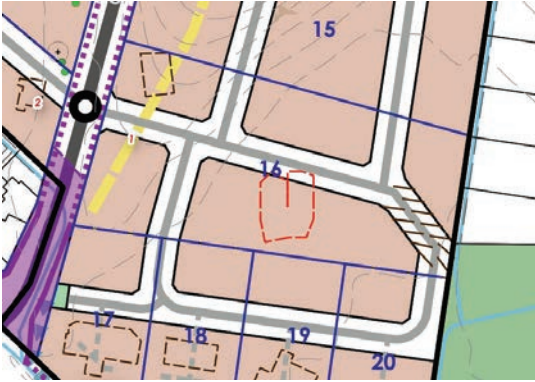
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16.15. Property 15 – 3 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 14 or Property 16. • Glismann Road (RD-01) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. • Shared Path to be constructed on Glismann Road. • Provide connections to Property 14 and Property 16.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Section 7 as relevant.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered. • Survey to determine areas of slope 20% and greater, with an appropriate design response.
Vegetation - Section 9	<ul style="list-style-type: none"> • Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Eastern coordination area.

16.16. Property 16 – 1 Glismann Road



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> Local Access Street RD-03 and DCP project LA-03. Glismann Road (RD-01 DCP project) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. The intersection with RD-01 must be designed to accommodate a future four-way roundabout with Property 1 The roundabout is to be constructed once access to Property 1 is constructed, unless otherwise agreed by the Responsible Authority. Design, construction and land costs for the roundabout are to be met by Property 1 and secured via a Section 173 Agreement as a condition of any permit for subdivision or development. Shared Path to be constructed on Glismann Road. Provide connections to Property 15, 17, 18 and 20. Refer to Table 9 for further information.
Land Use Typology - Section 6	<ul style="list-style-type: none"> Neighbourhood Residential
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> Refer to Section 7 as relevant.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> Slope to be considered.
Vegetation - Section 9	<ul style="list-style-type: none"> Removal of Habitat Zone is subject to a planning permit and detailed biodiversity assessment at the time of application. Refer to Section 9.1.
Landscape - Section 10	<ul style="list-style-type: none"> Consider interface to O'Neil Road Recreation Reserve.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> Refer to Section 11 as relevant.
Utilities - Section 12	<ul style="list-style-type: none"> Refer to Section 12 as relevant.
PRSA - Section 13	<ul style="list-style-type: none"> PRSA required. Refer to Section 13.
Development Coordination - Section 14	<ul style="list-style-type: none"> Southern coordination area.

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16.17. Property 17 – 111-113 Old Princes Highway



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Driveway access from Old Princes Highway, and/or street access via Property 16. • No through road will be permitted to Glismann Road for Property 18, 19 or Property 20 via Property 17. • Provision of an all movement intersection in this location may be possible subject to agreement from the Department of Transport and Planning and the Responsible Authority. Direct access to Glismann Road is otherwise restricted to left in/out operation only. • Construction of a roundabout to access Property 1 and Property 16 may likely provides alternative design solutions for Property 17. This would need to be discussed with relevant Property holders to facilitate the roundabout construction. • Glismann Road (RD-01) must be delivered concurrently from the Old Princes Highway for the extent of the Property frontage. • Shared Path to be constructed on Glismann Road. • Refer to Table 14 for further information.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Table 23 for further information.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.
Vegetation - Section 9	<ul style="list-style-type: none"> • Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> • Consider interface to the Old Princes Highway. Refer to Table 23 for further information.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain to front of property.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12 as relevant.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Southern coordination area.

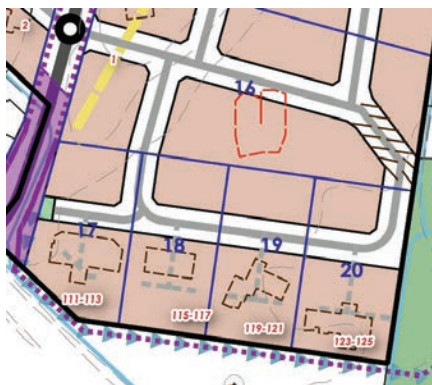
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16.18. Property 18 – 115-117 Old Princes Highway



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 16 or Property 18. • No access via Old Princes Highway is permitted once developed. Any existing access must be removed once developed. • No through road will be permitted to Glismann Road for Property 18, 19 or Property 20 via Property 17.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Table 23 for further information.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.
Vegetation - Section 9	<ul style="list-style-type: none"> • Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> • Consider interface to the Old Princes Highway. Refer to Table 23 for further information.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain to front of property.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12 as relevant.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Southern coordination area.

16.19. Property 19 – 119-121 Old Princes Highway



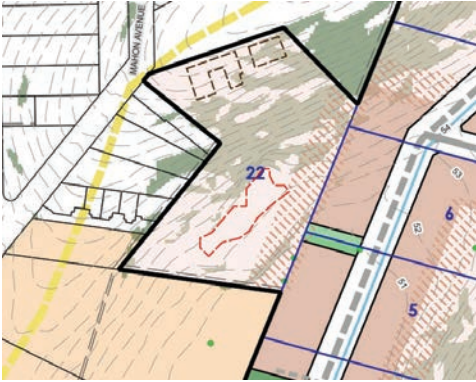
REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 18 or Property 20. • No access via Old Princes Highway is permitted once developed. Any existing access must be removed once developed. • No through road will be permitted to Glismann Road for Property 18, 19 or Property 20 via Property 17.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Table 23 for further information.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.
Vegetation - Section 9	<ul style="list-style-type: none"> • Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> • Consider interface to the Old Princes Highway. Refer to Table 23 for further information.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain to front of property.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12 as relevant.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Southern coordination area.

16.20. Property 20 – 123-125 Old Princes Highway



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Access to be provided via Property 16 or Property 19. • No access via Old Princes Highway is permitted once developed. Any existing access must be removed once developed. • No through road will be permitted to Glismann Road for Property 18, 19 or Property 20 via Property 17.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Neighbourhood Residential.
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Table 23 for further information.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Refer to Section 8 as relevant.
Vegetation - Section 9	<ul style="list-style-type: none"> • Refer to Section 9 as relevant.
Landscape - Section 10	<ul style="list-style-type: none"> • Consider interface to O'Neil Road Recreation Reserve. Refer to Section 10.2. • Consider interface to the Old Princes Highway. Refer to Table 23 for further information.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Accommodation of existing Melbourne Water drain to front of property.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12 as relevant.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Southern coordination area.

16.21. Property 22 – 11 Mahon Avenue



REFER TO GRADP:	PLUS SPECIFIC CONSIDERATIONS
Movement Network -Section 5	<ul style="list-style-type: none"> • Pedestrian connection to be provided via Mahon Avenue or Property 5 and 6.
Land Use Typology - Section 6	<ul style="list-style-type: none"> • Visually Sensitive Residential
Building Design Guidelines and Fencing Controls - Section 7	<ul style="list-style-type: none"> • Refer to Table 23 for further information.
Slope Management Guidelines - Section 8	<ul style="list-style-type: none"> • Slope to be considered.
Vegetation - Section 9	<ul style="list-style-type: none"> • Removal of Habitat Zone is subject to a planning permit and detailed biodiversity assessment at the time of application. Refer to Section 9.1.
Landscape - Section 10	<ul style="list-style-type: none"> • Refer to Section 10 as relevant.
Integrated Water Management - Section 11	<ul style="list-style-type: none"> • Refer to Section 11.
Utilities - Section 12	<ul style="list-style-type: none"> • Refer to Section 12.
PRSA - Section 13	<ul style="list-style-type: none"> • Not applicable.
Development Coordination - Section 14	<ul style="list-style-type: none"> • Refer to Section 14. No coordination area.

17. Attachments

17.1. Council Meeting Minutes - 17 November 2025

7 Ordinary Business

7.1 Town Planning Reports

7.1.1 GLISMANN ROAD AREA DEVELOPMENT PLAN APPROVAL	
Responsible GM:	Debbie Tyson
Staff Disclosure:	All officers involved in the preparation of this report have considered and determined that they do not have a conflict of interest in the matter.
Council Plan Reference:	<p>2. Vibrant Places</p> <p>2.1 Strategic planning - We ensure the Cardinia Shire Planning Scheme delivers high quality outcomes that preserve the liveability and identity of the municipality and protecting our natural environment, while also accommodating for population growth and economic development, while preserving the liveability and identity of the municipality and protecting our natural environment.</p> <p>3. Adaptive Environments</p> <p>3.1 Biodiversity - We protect our biodiversity and enhance our natural assets and diverse ecosystems.</p>

Resolution

Moved Cr Owen, seconded Cr Ross.

That Council:

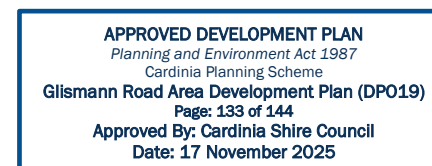
1. Approves the Glismann Road Area Development Plan (November 2025) (Attachment 3 to this report) under the provisions of Section 4.0 of Schedule 19 to Clause 43.04 Development Plan Overlay of the Cardinia Planning Scheme, and:
 - a. Approves minor administrative, formatting, and editorial changes to improve clarity and consistency throughout the document, provided such changes do not alter the strategic intent of the Development Plan.
 - b. Requires that any changes made to the document be recorded in an appendix to the approved and stamped Glismann Road Area Development Plan.
2. Notes the Engagement Outcomes Report (October 2025) (Attachment 4 to this report) and:
 - a. Thanks landowners and key internal and external stakeholders for their valuable participation and input during the informal engagement process.
 - b. Acknowledges the feedback received and its important role in shaping the Glismann Road Area Development Plan (GRADP).

For: Cr Kowarzik, Cr Nickell, Cr Owen, Cr Paton, Cr Pomeroy, Cr Potter, Cr Roberts, Cr Ross and Cr Thomsen
Against: Nil

Carried

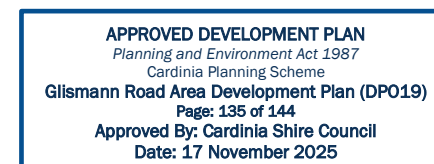
17.2. Appendix – Edits made to the Glismann Road Area Development Plan (November 2025) (Attachment 3 of Council Report of 17 November 2025)

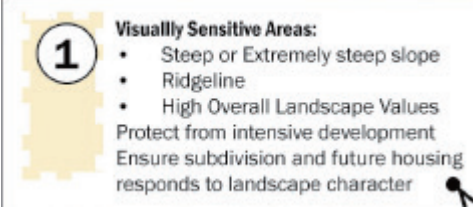
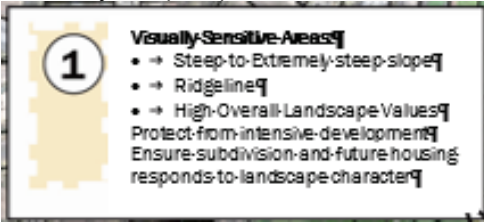

Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
Front	November 2025 (black font)	Front	<u>Amended Text</u> November 2025 (white font)
2 nd page	Version History Table	2 nd page	<u>Amended Text</u> Version History Table updated, Version: C , Date: 9 December 2025 Comment: Final Development Plan – Minor edits (see Section 17 Attachments) & Approved Stamp Author: CSC
Throughout document	Standard Page Footer	Throughout document	<u>Amended Format</u> <u>Increase Page Footer</u> to accommodate Approved Stamp.
Throughout document	Dot points	Throughout document	<u>Amended Format</u> <u>Updated formatting</u> to include expanded spacing between bullet points.
Throughout document	Tables	Throughout document	<u>Amended Format</u> <u>Updated formatting</u> to include expanded spacing between bullet points and paragraphs.



Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
Throughout document	Number of pages in the entire document 127	Throughout document	<u>Amended Format</u> Number of pages in the entire document 143 Formatting updates and this Appendix has resulted in an <u>overall increase in the total number of pages</u> in the document.
Throughout document	O'Neil Recreation Reserve	Throughout document	<u>Amend Text</u> O'Neil Road Recreation Reserve
Throughout document	Arboricultural assessment	Throughout document	<u>Amend Text</u> Arboricultural Arboriculture assessment
I - III	Table of Contents	I - III	<u>Amend Format</u> <u>Updated</u> to reflect change in page numbers
IV	Tables	IV	<u>Amend Format</u> <u>Updated</u> to reflect change in page numbers
V - VI	Figures	V - VI	<u>Amend Format</u> <u>Updated</u> to reflect change in page numbers
VII - VIII	Table 1 - Glossary of Terms used	VII - VIII	<u>Amend Text</u> Column One – Term Column Two – Definition

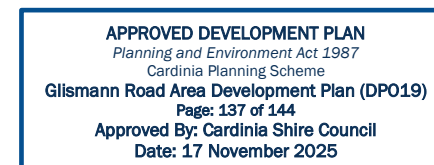
Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
1	Section 2 – Site Analysis: With reference to the Key Issues Paper (KIP), provides an overview of the existing site conditions with particular reference to topography and key constraints and opportunities.	2	<u>Amend Text</u> Section 2 – Site Analysis: With reference to the GRADP Key Issues Paper (KIP), provides an overview of the existing site conditions with particular reference to topography and key constraints and opportunities.
2	Section 15 – Land Budget: As provided in the Glismann Road Development Contributions Plan (DCP).	2	<u>Amend Text</u> Section 15 – Land Budget: As provided in the Glismann Road Development Contributions Plan (DCP).
11	3. Design Response 1) Visually sensitive areas – includes land with slopes assessed as Steep or Extremely Steep (refer to Table 4), land on or adjacent to ridgelines, and areas with high overall landscape values. These areas should be protected from intensive development, and future subdivision and housing designed to respond sensitively to topography, vegetation and landscape character.	12	<u>Amend Text</u> 3. Design Response 1) Visually sensitive areas – includes land with slopes assessed as Steep or to Extremely Steep (refer to Table 4), land on or adjacent to ridgelines, and areas with high overall landscape values. These areas should be protected from intensive development, and future subdivision and housing designed to respond sensitively to topography, vegetation and landscape character.
11	3. Design Response 3) Restricted access - areas along the Glismann Road (RD-02) crest, where access limitations are necessary to ensure safe vehicle movements for existing and future dwellings, and where the creation of new access points is restricted.	12	<u>Amend Text</u> 3. Design Response 3) Restricted access - areas along the Glismann Road (DCP Project RD-02) crest, where access limitations are necessary to ensure safe vehicle movements for existing and future dwellings, and where the creation of new access points is restricted.



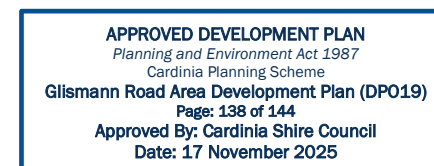
Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
11	<p>3. Design Response (last paragraph) The Design Response Plan should be read alongside DCP05, relevant DCP project sheets and subsequent sections of the GRADP to understand how infrastructure delivery and topographic constraints have been integrated into the overall design framework.</p>	12	<p><u>Amend Text</u></p> <p>3. Design Response (last paragraph) The Design Response Plan should be read alongside DCP05, relevant DCP project sheets (Glismann Road Development Contributions Plan Project Sheets, Cardinia Shire Council, November 2022), and subsequent sections of the GRADP to understand how infrastructure delivery and topographic constraints have been integrated into the overall design framework.</p>
12	<p>Figure 5 – Design Response Plan</p> 	13	<p><u>Amend Text</u></p> <p>Figure 5 – Design Response Plan, update to change to ‘Steep to Extremely steep slope’</p> 
13	<p>4.1 Vision Statement Glismann Road will become a welcoming and attractive neighbourhood that feels part of Beaconsfield while retaining its unique landscape character. New homes and streets will be carefully designed and sited to follow the natural shape of the land and protect the area’s landscape and views. The GRADP promotes a co-ordinated approach to development to deliver infrastructure, a diverse range of lot sizes, and attractive streetscapes and open spaces. Together these elements will create a connected neighbourhood that prioritises tree retention and values site responsive, high quality development outcomes.</p>	14	<p><u>Amend Format</u></p> <p>Modify from a standard paragraph to a defined box. No change to text content.</p> <p>4.1. Vision Statement</p> 

APPROVED DEVELOPMENT PLAN
 Planning and Environment Act 1987
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Glismann Road Area Development Plan (DP019)
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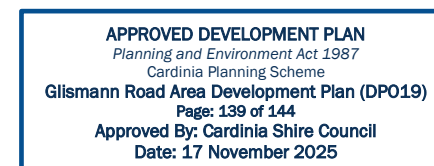
Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
18	<p>5.1. Primary Road Network</p> <p>The primary road network elements of the GRADP included in DCPO5 are:</p> <ul style="list-style-type: none"> • RD-01 – Glismann Road (Access Street Level 2): Old Princes Highway intersection to TM-01 (roundabout or alternative treatment to the satisfaction of the Responsible Authority). • RD-02 – Glismann Road (Access Street Level 1.5): TM-01 to truncation of road. • RD-03 – Local access road (Local Access Street Level 1) through Property 3 (6 Glismann Road). Includes a land component (LA-01). • RD-04 – Local access road (Local Access Street Level 1) through Property 8 (16 Glismann Road). Includes a land component (LA-02). • RD-05 – Local access road (Local Access Street Level 1) through Property 16 (1 Glismann Road). Includes a land component (LA-03). • TM-01 – Roundabout at the intersection of Glismann Road and RD-05 (Property 3, 6 Glismann Road). Includes a land component (LA-04a and LA-04b). 	20	<p><u>Amend Text</u></p> <p>5.1. Primary Road Network</p> <p>The primary road network elements of the GRADP included in DCPO5 are:</p> <ul style="list-style-type: none"> • DCP Project RD-01 (RD-01) – Glismann Road (Access Street Level 2): Old Princes Highway intersection to DCP Project TM-01 (TM-01) (roundabout or alternative treatment to the satisfaction of the Responsible Authority). • DCP Project RD-02 (RD-02) – Glismann Road (Access Street Level 1.5): TM-01 to truncation of road. • DCP Project RD-03 (RD-03)– Local access road (Local Access Street Level 1) through Property 3 (6 Glismann Road). Includes a land component (DCP Project LA-01 (LA-01)). • DCP Project RD-04 (RD-04)– Local access road (Local Access Street Level 1) through Property 8 (16 Glismann Road). Includes a land component (DCP Project LA-02 (LA-02)). • DCP Project RD-05 (RD-05) – Local access road (Local Access Street Level 1) through Property 16 (1 Glismann Road). Includes a land component (DCP Project LA-03 (LA-03)). • DCP Project TM-01 (TM-01) – Roundabout at the intersection of Glismann Road and RD-05 (Property 3, 6 Glismann Road). Includes a land component (DCP Project LA-04a (LA-04a) and DCP Project LA-04b (LA-04b)).
18	5.1.1. Glismann Road (RD-01 and RD-02)	21	<p><u>Amend Text</u></p> <p>5.1.1. Glismann Road (DCP Project RD-01 and DCP Project RD-02)</p>



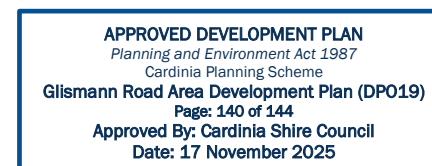
Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
18	5.1.1. Glismann Road (RD-01 and RD-02) 6 th paragraph Specifically, the first application to subdivide land with access to Glismann Road (Access Street Level 1.5) (which is RD-02) must include a FLP for the entire length of the road, or a length to the satisfaction of the Responsible Authority.	21	<u>Amend Text</u> 5.1.1. Glismann Road (RD-01 and RD-02) 6 th paragraph Specifically, the first application to subdivide land with access to Glismann Road (Access Street Level 1.5) (which is RD-02) must include a Functional Layout Plan (FLP) for the entire length of the road, or a length to the satisfaction of the Responsible Authority.
19	5.1.1. Glismann Road (RD-01 and RD-02) Last paragraph	21	<u>Amend Text</u> 5.1.1. Glismann Road (RD-01 and RD-02) Last paragraph Included hyperlink to the Glismann Road Traffic Impact Assessment Report, TrafficWorks (1 May 2022)
30	Table 15 - Western Loop Road – Local Access Street Level 1 (cross-section at Figure 13) – Design Requirements <ul style="list-style-type: none"> 20 m wide Local Access Street Level 1 that includes a 4m wide Melbourne Water easement on its eastern side (refer to Error! Reference source not found for cross-section). 	33	<u>Amend Text</u> Table 15 - Western Loop Road – Local Access Street Level 1 (cross-section at Figure 13) – Design Requirements <ul style="list-style-type: none"> 20 m wide Local Access Street Level 1 that includes a 4m wide Melbourne Water easement on its eastern side (refer to Error! Reference source not found for cross-section).Figure 13 - Western Loop Road cross-section – Access Street Level 1 (20m) with Melbourne Water).
30	Table 15 - Western Loop Road – Local Access Street Level 1 (cross-section at Figure 13) – On-street Parking <ul style="list-style-type: none"> On-street parking to be provided on both sides of the carriageway adjacent to the local open space LA-05a and LA-05b.. 	33	<u>Amend Text</u> Table 15 - Western Loop Road – Local Access Street Level 1 (cross-section at Figure 13) – On-street Parking <ul style="list-style-type: none"> On-street parking to be provided on both sides of the carriageway adjacent to the local open space (DCP Project LA-05a (LA-05a) and DCP Project LA-05b (LA-05b)).



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37	<p>5.7 Shared Path Network – Extent</p> <ul style="list-style-type: none"> • SPO1: located along the levee bank which runs along the frontage of Property 17 to 20, connecting through to O’Neil Road Recreation Reserve. • SPO1 in this section 	41	<p><u>Amend Text</u></p> <p>5.7 Shared Path Network – Extent</p> <ul style="list-style-type: none"> • DCP Project SP-01 (SP-01): located along the levee bank which runs along the frontage of Property 17 to 20, connecting through to O’Neil Road Recreation Reserve. • SP-01 in this section
67	<p>10.2 Open Space (1st paragraph)</p> <p>The GRADP also directly abuts O’Neil Recreation Reserve, with contributions towards reserve and pavilion upgrades also provided by DCPO5 Items (OSNR-01 and CI-01). Figure 36 identifies the location of open space within the GRADP.</p>	75	<p><u>Amend Text</u></p> <p>10.2 Open Space (1st paragraph)</p> <p>The GRADP also directly abuts O’Neil Road Recreation Reserve, with contributions towards reserve and pavilion upgrades also provided by DCPO5 (DCP Projects OSNR-01 and CI-01). Figure 36 identifies the location of open space within the GRADP.</p>
75	<p>11.2 Proposed Drainage system</p> <p>Paragraph after Figure 42.</p>	82	<p><u>Amend Format</u></p> <p>11.2 Proposed Drainage system</p> <p>Paragraph after Figure 42 moved to before Figure 42.</p>
78	<p>12.3 Electrical</p> <p>The Ausnet Services BYDA plan below shows existing 22kV distribution overhead powerlines along Princes Highway and extending halfway up Glismann Road.</p>	86	<p><u>Amend Text</u></p> <p>12.3 Electrical</p> <p>The Ausnet Services BYDA (Before You Dig Australia) plan below shows existing 22kV distribution overhead powerlines along Princes Highway and extending halfway up Glismann Road.</p>



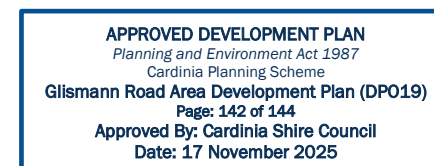
Original Page	Glismann Road Area Development Plan (November 2025) (Attachment 3 of 17 November 2025 Council Agenda)	New page number	Glismann Road Area Development Plan (November 2025) – Approved Stamp
79	12.4 Telecommunications A BYDA investigation confirms existing NBN infrastructure along Old Princes Highway and Glismann Road. Developers will be required to install pit and pipe infrastructure suitable for optical fibre installation and enter into a Master Development Agreement with NBN or other communications provider.	87	<u>Amend Text</u> 12.4 Telecommunications A BYDA (Before You Dig Australia) investigation confirms existing NBN (National Broadband Network) infrastructure along Old Princes Highway and Glismann Road. Developers will be required to install pit and pipe infrastructure suitable for optical fibre installation and enter into a Master Development Agreement with NBN or other communications provider.
85	15. Land Budget Paragraph after Table 27 As noted in Section 1.3, Property Reference 22, which is 11 Mahon Avenue is not included in the Development Contributions Plan but is included in the Development Plan. For further information regarding the exclusion of Property Reference 22 can be found in the Panel Report for Cardinia Planning Scheme Amendment C238card. There, Property 22 has not been included in the results in Table 27.	93	<u>Amend Text</u> 15. Land Budget Paragraph after Table 27 As noted in Section 1.3, Property Reference 22, which is 11 Mahon Avenue is not included in the Development Contributions Plan but is included in the Development Plan. For further information regarding the exclusion of Property Reference 22 can be found in the Panel Report for Cardinia Planning Scheme Amendment C238card. Therefore , Property 22 has not been included in the results in Table 27.
86 - 87	Footer – portrait setting	94 - 95	<u>Amend Format</u> Update Footer for landscape setting
89 – end of document	16. Individual Property Comments Footer content Glismann Road Area Development Plan – October 2025	97 – end of document	<u>Amend Text</u> 16. Individual Property Comments Footer content Glismann Road Area Development Plan – November 2025



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89 to 115	16. Individual Property Comments Image located at the end of the table for each Property.	97 - 123	<u>Amend Format</u> 16. Individual Property Comments Image <u>moved to the front</u> of the table for each Property.
92	16.3 Property 3 – 6 Glismann Road Movement Network – Section 5 • Refer to Table 11 for further information.	100	<u>Amend Text</u> 16.3 Property 3 – 6 Glismann Road Movement Network – Section 5 • Refer to Table 10 for further information.
96	16.5 Property 5 – 10 Glismann Road Movement Network – Section 5 • Provide connections to Property 4, Property 6 and Property 22.	104	<u>Amend Text</u> 16.5 Property 5 – 10 Glismann Road Movement Network – Section 5 • Provide connections to Property 4, Property 6 and a pedestrian connection to Property 22.
97	16.6 Property 6 – 12 Glismann Road Movement Network – Section 5 • Provide connections to Property 4, Property 6 and Property 22.	105	<u>Amend Text</u> 16.6 Property 6 – 12 Glismann Road Movement Network – Section 5 • Provide connections to Property 4, Property 6 and a pedestrian connection to Property 22.
99	16.8 Property 8 – 16 Glismann Road Movement Network – Section 5 • Refer to Table 8Table 7 for further information.	107	<u>Amend Text</u> 16.8 Property 8 – 16 Glismann Road Movement Network – Section 5 • Refer to Table 8 Table 7 for further information.



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105	16.14 Property 14 – 5 Glismann Road Movement Network – Section 5 <ul style="list-style-type: none"> Refer to Table 12Table 7 for further information 	113	<u>Amend Text</u> 16.14 Property 14 – 5 Glismann Road Movement Network – Section 5 <ul style="list-style-type: none"> Refer to Table 12Table 7 for further information
108	16.16 Property 16 – 1 Glismann Road Landscape – Section 10 <ul style="list-style-type: none"> Consider interface to reserve at truncation of Glismann Road. 	116	<u>Amend Text</u> 16.16 Property 16 – 1 Glismann Road Landscape – Section 10 Consider interface to O’Neil Road Recreation Reserve.
110	16.17 Property 17 – 111-113 Old Princes Highway Movement Network – Section 5 <ul style="list-style-type: none"> No access via Old Princes Highway is permitted once developed. Any existing access must be removed once developed. Provision of an all movement intersection in this location may be possible subject to agreement from the Department of Transport and Planning and the Responsible Authority. Direct access is otherwise restricted to left in/out operation only. 	117	<u>Amend Text</u> 16.17 Property 17 – 111-113 Old Princes Highway Movement Network – Section 5 <ul style="list-style-type: none"> No access via Old Princes Highway is permitted once developed. Any existing access must be removed once developed. Provision of an all movement intersection in this location may be possible subject to agreement from the Department of Transport and Planning and the Responsible Authority. Direct access to Glismann Road is otherwise restricted to left in/out operation only.
	17. Attachment A Council Meeting – 17 November 2025 Minutes		<u>Amend Text</u> 17. Attachments 17.1 Council Meeting Minutes – 17 November 2025 Minutes 17.2 Appendix – Edits made to the Glismann Road Area Development Plan (November 2025) (Attachment 3 of Council Report of 17 November 2025)



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